



DECISIONS OF THE QUEENSTOWN LAKES DISTRICT COUNCIL
NOTIFICATION UNDER s95A AND s95B AND DETERMINATION UNDER s104
OF THE RESOURCE MANAGEMENT ACT 1991

Applicant:	C & J Limited
RM reference:	RM180133
Application:	Application under Section 88 of the Resource Management Act 1991 (RMA) for a land use consent to construct and operate a two-storey motel complex comprising 21 studio visitor accommodation units, with associated site and zone standard breaches relating to road setback, earthworks, boundary planting, building height, recession plane, and transport standards.
Location:	86 Brownston Street / 33 Dungarvon Street, Wanaka
Legal Description:	Section 1 Block XXIII Town of Wanaka held in Record of Title OT332/41
Zoning:	ODP: Low Density Residential PDP: Medium Density Residential
Activity Status:	Non-complying
Date	17 November 2020

SUMMARY OF DECISIONS

1. Pursuant to sections 95A-95F of the Resource Management Act 1991 (**RMA**) the application will be processed on a **non-notified** basis given the findings of Section 5 of this report. This decision is made by Sarah Gathercole, Senior Planner, on 17 November 2020 under delegated authority pursuant to Section 34A of the RMA.
2. Pursuant to Section 104 of the RMA, consent is **GRANTED SUBJECT TO CONDITIONS** outlined in **Appendix 1** of this decision imposed pursuant to Section 108 of the RMA. This consent can only be implemented if the conditions in Appendix 1 are complied with by the consent holder. The decision to grant consent was considered (including the full and complete records available in Council's electronic file and responses to any queries) by Sarah Gathercole, Senior Planner as delegate for the Council.

1. SUMMARY OF PROPOSAL AND SITE DESCRIPTION

Consent is sought to establish a new two-storey motel complex comprising of 21 studio units at 86 Brownston Street in Wanaka. The ground floor will comprise eleven visitor accommodation units over the building footprint area of 325m². On the first floor, there will be ten visitor accommodation units, an office and a laundry. The gross floor area of the building will be approximately 618m², which equates to a site coverage of approximately 32.5% as the site area is 1,002m². The maximum building height is approximately 8.15m, whilst the lower part of the roofline is approximately 6m above ground level.

The proposed development breaches a number of site and zone standards relating to road setback, boundary planting, building height, recession plane, and parking.

The existing vehicle crossing to Dungarvon Street is proposed to be relocated further away from the Dungarvon Street and Brownston Street intersection. 11 on-site parking spaces are proposed.

No signage is proposed.

The applicant has provided a detailed description of the proposal, the site and locality and the relevant site history in Sections 1-3 of the report entitled '*Application for Resource Consent to the Queenstown Lakes District Council – C&J Limited – To establish and operate a motel at 86 Brownston Street, Wanaka – 29 January 2018,*' prepared by Anita Collie of Town Planning Group, and submitted as part of the application (hereon referred to as the applicant's AEE and attached as Appendix 2). This description is considered accurate and is adopted for the purpose of this report.

Figure 1 below provides an aerial image of the subject site and surrounding area:



Fig. 1 Subject site and surrounding environment

2. ACTIVITY STATUS

The proposal requires consent for the following reasons:

2.1 OPERATIVE DISTRICT PLAN (ODP)

The subject site is zoned Low Density Residential in the ODP and the proposed activity requires resource consent for the following reasons:

- A **restricted discretionary** activity pursuant to Rule 22.3.2.3 as the proposal breaches site standard 22.3.3(ii)(b)(iii) in regard to the distance of cuts from the boundary. It is proposed to undertake earthworks up to the boundary of the site. The proposed building will be set back 2.6m from the western boundary. Council's discretion is restricted to:
 - The nature and scale of the earthworks
 - Environmental protection measures
 - Remedial works and revegetation
 - The effects on landscape and visual amenity values
 - The effects on land stability and flooding
 - The effects on water bodies
 - The effects on cultural and archaeological sites
 - Noise
- A **restricted discretionary** activity pursuant to Rule 14.2.2.3(ii) as the proposal breaches site standard 14.2.4.1(v) in regard to aisle widths. The proposed aisle width (5.7m) of the car park is narrower than required for Class 2 users. Council's discretion is restricted to this matter.

For the avoidance of doubt, the following rules are relevant to the application but are treated as inoperative under s86F of the RMA:

- A **controlled** activity pursuant to Rule 7.5.3.2 (iii)(a) Buildings for Non-Residential Activities as the proposed building will be used for visitor accommodation purposes.
- A **restricted discretionary** activity pursuant to Rule 7.5.3.4(i) Visitor Accommodation Located in the Low Density Residential Zone, in respect of:
 - a) The location, external appearance and design of buildings;
 - b) The location, nature and scale of activities on-site;
 - c) The location of parking and buses and access;
 - d) Noise, and
 - e) Hours of operation
- A **restricted discretionary** activity pursuant to Rule 7.5.3.4(vi) as the proposal breaches site standard 7.5.6.2(iii)(f) in regard to the 4m minimum internal setback for visitor accommodation activities. The proposed building will be set back 2.6m from the south-western boundary. Council's discretion is restricted to this matter.
- A **restricted discretionary** activity pursuant to Rule 7.5.3.4(vi) as the proposal breaches site standard 7.5.5.2(iii) in regard to the 4.5m minimum road setback. The proposed building will be set back 2m from both Dungarvon Street and Brownston Street. Council's discretion is restricted to this matter.
- A **restricted discretionary** activity pursuant to Rule 7.5.3.4(vi) as the proposal breaches site standard 7.5.5.2(xiii) in regard to the 1.9m maximum height of boundary planting within 2m of the boundary. An existing hedge along the south east boundary exceeds 1.9m in height, as will proposed trees along the Dungarvon Street frontage. Council's discretion is restricted to this matter.

- A **non-complying** activity pursuant to Rule 7.5.3.5 as the proposal breaches zone standard 7.5.5.3(v) relating to maximum building height and recession planes. With a maximum height of 8.15m, the building breaches the 7m maximum height limit. The proposal also breaches the recession planes at the north west corner of the site at approximately 4m.
- A **restricted discretionary** activity pursuant to Rule 14.2.2.3(ii) as the proposal breaches site standard 14.2.4.1(i) in regard to the minimum number of car parks required, which is 42. It is proposed to provide 11 car parks. Council's discretion is restricted to this matter.

2.2 PROPOSED DISTRICT PLAN (PDP)

Council notified its decisions on Stage 1 of the Proposed District Plan 5 May 2018 and notified decisions on Stage 2 of the Proposed District Plan on 21 March 2019. The subject site is zoned Medium Density Residential by the PDP (Stage 1) and the proposed activity requires resource consent for the following reasons:

Rules that are treated as operative under s86F:

- A **non-complying** activity pursuant to Rule 8.4.17 for visitor accommodation not otherwise identified.
- A **restricted discretionary** activity pursuant to Rule 8.5.6.2 as the proposed building will exceed the height recession plane along the western elevation by a height of approximately 3.3m and an area of approximately 7.6m². Council's discretion is restricted to:
 - Any sunlight, shading or privacy effects created by the proposal on adjacent sites and/or their occupants;
 - Effects on any significant public views (based on an assessment of public views undertaken at the time of the proposal, in addition to any specified significant public views identified within the District Plan);
 - External appearance, location and visual dominance of the building(s) as viewed from the street(s) and adjacent properties;
- A **restricted discretionary** activity pursuant to Rule 8.5.7 in relation to landscaped permeable surface, which is required to be at least 25%. It is proposed to provide 16% landscaped permeable surfacing. Council's discretion is restricted to:
 - stormwater related effects including flooding and water nuisance;
 - visual amenity and the mitigation of the visual effects of buildings and any vehicle parking areas, particularly in relation to any streets or public spaces;
- A **restricted discretionary** activity pursuant to Rule 8.5.8(a) as the proposed building will be constructed 1m into the 3m minimum road boundary setback. Council's discretion is restricted to:
 - External appearance, location and visual dominance of the building(s) as viewed from the street(s) and adjacent properties; streetscape character and amenity;
 - Any sunlight, shading or privacy effects created by the proposal on adjacent sites and/or their occupants;
 - Effects on any significant public views (based on an assessment of public views undertaken at the time of the proposal, in addition to any specified significant public views identified within the District Plan);
 - Parking and access layout: safety, efficiency and impacts on on-street parking and neighbours;
- A **restricted discretionary** activity pursuant to Rule 8.5.9 as the proposed façade of the building will be longer than 24m. The northern façade of the building is proposed to be 33m in length. Council's discretion is restricted to:

- External appearance, location and visual dominance of the building(s) as viewed from the street(s) and adjacent properties.
- A **non-complying** activity pursuant to Rule 8.5.1.1 as the proposed building would exceed the maximum height limit of 7m by 1.15m.
- A **restricted discretionary** activity pursuant to Rule 29.5.1 as the proposal breaches standard 29.8.11 in regard to the minimum parking requirements, which is 15. It is proposed to provide 11 car parks. Council's discretion is restricted to: a. The number of parking spaces provided, and b. The allocation of parks to staff/ guests and residents/ visitors.
- A **restricted discretionary** activity pursuant to Rule 29.5.18 Minimum Sight Distances from Vehicle Access on all roads other than State Highways as the sight distance available from the vehicle crossing to the northwest is approximately 66m, less than the required 80m. Council's discretion is restricted to effects on safety, efficiency, and amenity of the site and of the transport network, including the pedestrian and cycling environment.

Rules that have legal effect under s86F but are not yet treated as operative due to appeals are:

- A **restricted discretionary** activity pursuant to Rule 25.5.18 as the proposed earthworks include cuts of greater than 0.3m adjacent to the boundary of the lot. Council's discretion is restricted to this matter.
- A **restricted discretionary** activity pursuant to Rule 29.5.3 size of parking spaces and layout, as the proposal breaches Table 29.11 in regard to aisle width. The proposed aisle width (5.7m) of the car park is narrower than required for Class 2 users. Council's discretion is restricted to the size and layout of parking spaces and associated manoeuvring areas.

2.3 ACTIVITY STATUS

As the application was lodged prior to 21 March 2019 when the decisions on the PDP visitor accommodation provisions were notified (included in Stage 2 of the PDP), the activity status continues to be processed, considered, and decided as an application for the type of activity that it was for, or was treated as being for, at the time the application was first lodged.

Overall, the application is considered to be a **non-complying** activity under the ODP and the PDP.

2.4 NATIONAL ENVIRONMENTAL STANDARD FOR ASSESSING AND MANAGING CONTAMINANTS IN SOIL TO PROTECT HUMAN HEALTH

Based on the applicant's review of Council records, the piece of land to which this application relates is not a HAIL site, and therefore the NES does not apply.

3. SECTION 95A – PUBLIC NOTIFICATION

Section 95A of the RMA requires a decision on whether or not to publicly notify an application. The following steps set out in this section, in the order given, are used to determine whether to publicly notify an application for a resource consent.

3.1 Step 1 – Mandatory public notification

The applicant has not requested public notification of the application (s95A(3)(a)).

Public Notification is not required as a result of a refusal by the applicant to provide further information or refusal of the commissioning of a report under section 92(2)(b) of the RMA (s95A(3)(b)).

The application does not involve exchange to recreation reserve land under section 15AA of the Reserves Act 1977 (s95A(3)(c)).

Therefore, public notification is not required by Step 1.

3.2 Step 2 – Public notification precluded

Public notification is not precluded by any rule or national environmental standard (s95A(5)(a)).

The proposal is for a non-complying activity not otherwise defined; therefore, public notification is not precluded (s95A(5)(b)(i)-(iv)).

Therefore, public notification is not precluded by Step 2.

3.3 Step 3 – If not precluded by Step 2, public notification is required in certain circumstances

Public notification is not specifically required under a rule or national environmental standard (s95A(8)(a)).

A consent authority must publicly notify an application if notification is not precluded by Step 2 and the consent authority decides, in accordance with s95D, that the proposed activity will have or is likely to have adverse effects on the environment that are more than minor (s95A(8)(b)).

An assessment in this respect is therefore made in section 3.3.1 – 3.3.4 below:

3.3.1 Effects that must be disregarded (s95D(a)-(e))

- A: *Effects on the owners or occupiers of land on which the activity will occur and on adjacent land (s95D(a)).*
- B: *Trade competition and the effects of trade competition (s95D(d)).*
- C: *The following persons have provided their **written approval** and as such adverse effects on these parties have been disregarded (s95D(e)).*

Person (owner/occupier)	Address (location in respect of subject site)
New Zealand Trustee Services Limited, Noeline Boet and Ronald Boet	37 Dungarvon Street (Lot 1 DP 27737)



Figure 2 – Location of written approval: application site outlined in blue, written approval depicted by red star

Effects that may be disregarded:

D: An adverse effect of the activity if a rule or national environmental standard permits an activity with that effect (s95D(b), a permitted baseline assessment is undertaken (if applicable) in section 3.3.2 below).

3.3.2 Permitted Baseline (s95D(b))

The consent authority **may** disregard an adverse effect of the activity if a rule or national environmental standard permits an activity with that effect.

Operative District Plan

The permitted baseline would include two residential units built to comply with the Site and Zone Standards (including earthworks). Under the ODP definition for visitor accommodation, residential units can be used for visitor accommodation for one household of visitors (meaning a group which functions as one household) for a minimum stay of 3 consecutive nights up to a maximum (i.e. single let or cumulative multiple lets) of 90 nights per calendar year as a Registered Holiday Home. These provisions form part of a limited permitted baseline for which adverse effects can be considered against.

Proposed District Plan

The permitted baseline would include three residential units built to comply with the relevant standards. Under the PDP the residential units can be used for one Homestay not exceeding 5 paying guests per site that complies with minimum parking requirements and do not generate any heavy vehicle movements. These provisions form part of a limited permitted baseline against which adverse effects can be considered.

3.3.3 Receiving Environment

The immediately adjoining property to the south-west is occupied by a visitor accommodation activity (YHA Wanaka) that is zoned Medium Density Residential with a Visitor Accommodation Sub-Zone overlay. The block to the north-east is zoned Medium Density Residential with a Town Centre Transition Overlay; although it is noted that this overlay is under appeal. This is considered to form part of the receiving environment.

3.3.4 Assessment: Effects On The Environment

Taking into account sections 3.3.1 - 3.3.3 above, the following assessment determines whether the proposed activity will have, or is likely to have, adverse effects on the environment that are more than minor that will require public notification (s95A(8)(b)).

Council's Resource Management Engineer, Mr Cam Jones, has made an assessment of the proposal in terms of engineering matters. Mr Jones' report has been adopted for the purposes of this assessment and is attached as Appendix 3.

Council's Consultant Urban Designer, Ms Erin Quin, has made an assessment of the proposal in terms of design. Ms Quin's report has been adopted for the purposes of this assessment and is attached as Appendix 4.

Council's Consultant Acoustic Engineer, Dr Stephen Chiles, has made an assessment of the proposal in terms of noise. Dr Chiles' report has been adopted for the purposes of this assessment and is attached as Appendix 5.

The Assessment of Effects provided at section 5 of the applicant's AEE, is comprehensive and is considered accurate. It is therefore adopted for the purposes of this report, with the following additions:

Operative District Plan

The relevant assessment matters are found in Sections 14.3.2[iii] of Chapter 14 [Transport] and Section 22.4 of Chapter 22 [Earthworks] of the ODP. These matters have been considered in the assessment below.

Parking design

The applicant proposes to seal the parking area, and Mr Jones is satisfied that compliance with Council's requirements for surfacing and gradients will be achieved. Mr Jones recommends conditions in regard to detailed design and formation.

Overall, it is considered that any adverse effects in regard to the parking design will be less than minor.

Parking shortfall

The parking shortfall under the ODP is substantial, with 11 parks provided compared with the required 42. In this instance, however, it is considered unlikely that studio visitor accommodation units with one bed would typically generate two vehicles each. In addition, the receiving environment for the PDP zoning and associated transport standards is now considered more relevant, as the zoning and transport standard 29.8.11 (in regard to minimum parking requirements), are beyond appeal.

As such, it is considered that when compared with the receiving environment, any adverse effects in regard to the parking shortfall would be no more than minor.

Aisle width

The layout of the proposed car park was amended to increase the aisle width to 5.7m wide and increase the width of the parking spaces to 2.8m. The applicant provided swept paths confirming that the combination of a 5.7m aisle plus a 2.8m wide parking space provides sufficient manoeuvring area to allow a car to be driven into the space without reversing, and that only one reverse movement is required upon exiting the space. In addition, a turning bay is provided for the situation where a car enters the car park, finds it full, and needs to exit the site without reversing. Mr Jones has assessed the aisle width breach and applicant's assessment prepared by Carriageway Consulting Ltd (CCL).

Mr Jones notes that an unassigned area towards the north of the parking area has been provided, so that a vehicle can turn around in the event that there are no spare spaces in the parking area and leave onto Dungarvon Street forwards. Mr Jones recommends that this area be clearly and permanently marked out as a no parking area.

Overall, it is considered that adverse effects in regard to the aisle width breach would be less than minor.

Earthworks

Excavation is proposed to lower the finished floor level of the motel. This is to be retained along the north, east and south boundaries.

A maximum cut depth of up to 0.8m is proposed on the east and south boundaries, with permanent retaining walls proposed. The applicant has provided advice from Geotek Consulting indicating that this cut will not be stable, and temporary retaining is therefore required. A temporary retaining design and construction methodology has been provided.

Mr Jones accepts this expert advice, and recommends conditions requiring that the works be supervised by a suitably qualified person, and that the construction of the retaining walls be undertaken in accordance with this methodology.

A maximum cut depth of up to 1.45m is proposed along the east (Dungarvon Street) boundary, with permanent retaining walls proposed. This will require temporary batter slopes into the Council's road reserve, which will require a temporary Licence to Occupy (LTO) as the duration of the works is fairly long (indicated to be 2-3 weeks). The applicant has volunteered a condition that an appropriate LTO be

procured prior to the commencement of works, and Mr Jones recommends that this be provided as part of the Engineering Acceptance application.

As the earthworks are in close proximity to neighbouring properties, Mr Jones recommends an advice note with regard to undertaking a preconstruction survey.

Further conditions are recommended in regard to retaining walls, batter slopes, temporary retaining and engineering supervision, site management, construction crossing and site rehabilitation.

In regard to potential adverse effects on neighbouring sites as a result of the proximity of the works to boundaries and unstable gravels, Mr Jones is satisfied that the earthworks are feasible subject to compliance with all recommended conditions.

Given all of the above, it is considered that adverse effects in regard to earthworks would be no more than minor.

Services

The existing residential unit is connected to all services except stormwater. Mr Jones recommends conditions in regard to a larger water connection, confirming or upgrading the wastewater connection, and stormwater soakage design.

Overall, it is considered that adverse effects in regard to services would be less than minor.

Summary

Overall, when assessed under the provisions of the ODP, it is considered that adverse effects of the proposed development on the wider environment would be no more than minor.

Proposed District Plan

The relevant assessment matters are found in Chapters 8 [Medium Density Residential], 25 [Earthworks], 29 [Transport] of the PDP. These matters have been considered in the assessment below.

Acoustic effects

Dr Chiles recommended that a 2m acoustic fence or wall should be constructed along the boundary of the car park and the adjoining sites at 37 Dungarvon and 90 Brownston Streets to ensure that noise complies with the night-time standards. A condition in this respect is recommended.

Dr Chiles provided additional recommendations in regard to noise mitigation, and concluded that, subject to conditions, noise should comply with the limits and be acceptable in this environment.

Overall, adverse effects in regard to noise are considered to be no more than minor.

Urban design

Ms Quin has undertaken a comprehensive review of the proposal, including building design, site layout and parking and associated breaches relating to road setback, boundary planting, building height, and recession planes. The building has been architecturally designed to address the coherence and attractiveness from street and adjoining private viewpoints. The applicant's architect has intentionally aimed to address the corner of the site as a feature of the design; however, landscaping is proposed to help soften the appearance of the building from public view points. Given the proximity to the town centre and the variation of residential built form in the locale, the proposed development is considered to be compatible with the appearance, layout and scale of other buildings in the surrounding area.

Following design changes, Ms Quin identified three outstanding areas of concern in regard to screening ('fins'), glazing, and landscaping.

Screening/fins

The applicant is of the view that the 'fins' are an integral part of the architectural building design, and removing them creates a larger area of blank wall; however, the applicant also suggested that these features could be reduced in bulk (for example 30% reduction in height). This suggestion is accepted and a condition in this respect is recommended.

Glazing

The applicant has confirmed that all doors fronting the car park will have glazed vision panels to provide informal surveillance as well as privacy. A condition in this respect is recommended.

Landscaping

Ms Quin has recommended changes to the landscaping to improve street amenity. Conditions are therefore recommended in regard to relocating the road frontage fencing to behind the plantings and reducing the height or increasing the visual permeability of this fencing, ensuring that trees are of an adequate size at planting, and providing non-slip surfaces.

Summary

Given the above, it is considered that the building design, site layout and parking is appropriate for the site and surrounding area. Overall, adverse effects in regard to urban design are considered to be no more than minor.

Visitor accommodation

Visitor accommodation located outside of the Visitor Accommodation Sub-Zones and the Wanaka Town Centre Transition Overlay is restricted. Although the application is for a new build visitor accommodation operation, it is considered that the activity is appropriate on this site and zone, being in close proximity to town centre amenities, recreation areas and transport connections. As such, the loss of existing and future residential use of this site is acceptable.

As discussed further above, the building design and site layout is appropriate and will provide for good urban design outcomes. Hours of operation and noise associated with visitor accommodation activities will be actively managed onsite and will be mitigated by acoustic structures. An appropriate level of visitor parking is provided and there are viable options for alternative transport means, as discussed further below. The site layout will ensure ongoing privacy for adjoining neighbours, noting that one neighbour is also a visitor accommodation development.

In accordance with s88A(1A) the application continues to be processed, considered, and decided as an application for the type of activity that it was for, or was treated as being for, at the time the application was first lodged'. As the ODP activity status is 'frozen' under s88A(1A) the visitor accommodation aspect of the application has been considered as a non-complying activity within the Low Density Residential zone.

Having had regard to both the ODP and PDP for the above assessment, it is considered the adverse effects of the proposed visitor accommodation on the environment to be minor or less than minor.

Vehicle crossings/access

The applicant is proposing the construction of a new vehicle crossing towards the south end of the Dunganon Street frontage. Mr Jones is satisfied that compliant break over angles can be achieved, and that the crossing is an appropriate length at 5.5m.

Mr Jones notes that the sight distance available from the vehicle crossing to the northwest is approximately 66m, less than the 80m required for non-residential activities. The applicant has provided an assessment of this breach from CCL, which states that the sight distance is restricted by the intersection of Brownston Street / Dunganon Street. As vehicles negotiating this intersection will have

slowed down, CCL considers that the available sight distance is appropriate. Mr Jones agrees with this expert advice.

As the existing vehicle crossing will no longer be required, Mr Jones recommend a condition that it be removed and grassed berm reinstated, as well as conditions in regard to detailed design of the new vehicle crossing and removal of the existing vehicle crossing.

Overall, it is considered that adverse effects in regard to access and sight distance breach would be no more than minor.

Permeable surface

The applicant proposes to provide 16% permeable landscaped surface where at least 25% is required.

In regard to visual amenity, it is considered that adverse effects of less landscaping would be mitigated by the building design and the parking area being set back from the street away from public views.

In regard to stormwater related effects including flooding and water nuisance, Mr Jones understands that there is good soakage available in the gravels in the area. As noted above, Mr Jones recommends a condition that the detailed design of the soakage proposed be provided to Council for Engineering Acceptance prior to the commencement of works.

Given the above, adverse effects in regard to the permeable surface shortfall is considered to be no more than minor.

Parking shortfall

11 car parks are provided compared with the required 15. The applicant proposes to manage car parking by allocating spaces to a specific guest. Guests will book the car parking space for a small additional charge at the time that they book the room. If there is no car parking available on-site when a potential guest requests accommodation, they will be informed that there are no on-site spaces available, and be informed of other local parking or transport options or other accommodation options. If there are no parking spaces available, which may occur when occupancy rates are high, customers may choose an alternative accommodation option with available parking, or alternative transport.

In regard to alternative transport options, the site is located very close to the Dungarvon Street bus stop (approx. 95m along easy grade footpaths with a pedestrian refuge on Brownston Street in front of the site). There are regular shuttles to and from Queenstown Airport, tramping tracks, and intercity services. The site is located close to active transport routes to the Wanaka Town Centre, the lakefront, and other amenities. It is therefore considered that alternative transport is a viable option for this site.

While the 21 units are self-contained and will be used individually for visitor accommodation there will be the potential for them to be let to one group. Such a booking could lead to the potential for coach drop off or pick up from the site. However, given the constrained nature of the site and the residential nature of both Dungarvon and Brownstown Streets it will not be desirable for drop off or pick up via a coach. As noted above, a coach park is located close by. It is recommended that that the drop-off and pick-up at the site via coach will not be allowed.

The management booking method, in combination with alternative transport availability, is considered to mitigate adverse effects resulting from the parking shortfall to an acceptable level. Therefore, it is considered adverse effects in relation to parking will be no more than minor.

3.3.5 Decision: Effects On The Environment (s95A(8))

On the basis of the above assessment, it is considered that the proposed activity is not likely to have adverse effects on the environment that are more than minor. Therefore, public notification is not required under Step 3.

3.4 Step 4 – Public Notification in Special Circumstances

There are no special circumstances in relation to this application.

4. LIMITED NOTIFICATION (s95B)

Section 95B(1) requires a decision on whether there are any affected persons (under s95E). The following steps set out in this section, in the order given, are used to determine whether to give limited notification of an application for a resource consent, if the application is not publicly notified under section 95A.

4.1 Step 1: certain affected groups and affected persons must be notified

Determination under s95B(2)

The proposal does not affect protected customary rights groups, and does not affect a customary marine title group; therefore limited notification is not required.

Determination under s95B(3)

Limited notification is not required under Step 1 as the proposal is not on or adjacent to, or may affect land subject to a statutory acknowledgement under Schedule 11, and the person to whom the statutory acknowledgement is made is not determined an affected person under section 95E (s95B(3)).

4.2 Step 2: if not required by Step 1, limited notification precluded in certain circumstances

Limited notification is not precluded under Step 2 as the proposal is not subject to a rule in the District Plan or is not subject to a NES that precludes notification (s95B(6)(a)).

Limited notification is not precluded under Step 2 as the proposal is not a controlled activity.

4.3 Step 3: if not precluded by step 2, certain other affected persons must be notified

If limited notification is not precluded by step 2, a consent authority must determine, in accordance with section 95E, whether the following are affected persons:

Boundary activity

The proposal is not a boundary activity where the owner of an infringed boundary has not provided their approval.

Any other activity

As limited notification is not required under s95B(7), the proposed activity falls into the ‘*any other activity*’ category (s95B(8)), and the adverse effects of the proposed activity are to be assessed in accordance with section 95E.

4.3.1 Considerations in assessing adverse effects on Persons (S95E(2)(a)-(c))

- a) The consent authority **may** disregard an adverse effect of the activity on a person if a rule or national environmental standard permits an activity with that effect. In this case the permitted baseline is found within section 3.3.2 above and is of some relevance.
- b) The consent authority **must** disregard an adverse effect of the activity on the person if the effect does not relate to a matter for which a rule or a national environmental standard reserves control or restricts discretion; and
- c) The consent authority **must** have regard to every relevant statutory acknowledgement specified in Schedule 11.

4.3.1 [iii] Persons who have provided written approval (s95E(3))

The persons identified in Section 3.3.1 above have provided their **written approval** and as such adverse effects on these parties are disregarded for the purpose of s95E(3).

4.3.2 Assessment: Effects on Persons

Taking into account the exclusions in section 95E(2) and (3) as set out in section 4.3.1 above, the following outlines an assessment as to whether the activity will have or is likely to have adverse effects on persons that are minor or more than minor:

The nearest neighbouring properties are identified on Figure 1 above with green diamonds.

88 Brownston St: Secs 2-3 13 Blk XXIII Wanaka TN

This site is occupied by the Wanaka YHA and contains existing buildings and hedges close to along the south-west boundary. Views into the application site would be limited. Adverse effects in regard to dominance would be mitigated by building setbacks and design. Noise and nuisance will be managed onsite and by conditions of consent in regard to hours of operation. Mr Jones is satisfied that no instability on neighbouring sites will result from site works and temporarily retaining.

Overall, it is considered that adverse effects on owners and occupiers of this site would be less than minor.

82 Brownston St: Lot 1 DP 11074

This site is occupied by a residential unit. Views to application site would be mitigated by separation distance across Dungarvon Street. Noise and nuisance will be managed onsite and by conditions of consent in regard to hours of operation. Overall, it is considered that adverse effects on owners and occupiers of this site would be less than minor.

Pembroke Park

This site is used for recreational purposes with a car park being the closest part of the park to the application site. Visual effects would be mitigated by separation distance across Brownston Street. Noise and nuisance will be managed onsite and by conditions of consent in regard to hours of operation. It is considered that any adverse effects on park users would be less than minor.

Other persons

The building design and site layout is considered acceptable and will result in good urban design outcomes. Adverse effects on privacy and visual amenities have been identified and mitigated to acceptable levels. Noise and nuisance associated with construction will be managed in accordance with Council standards and conditions of consent. Therefore, adverse effects from the building design and construction on owners and occupiers of adjoining sites, and other persons, are considered to be less than minor.

Being close to the Wanaka Town Centre and other amenities, the visitor accommodation proposal is considered appropriately located. Alternative transport options are available, and sufficient parking and manoeuvring space is available for the guests, such that road and pedestrian safety will be maintained. The scale of the activity for 21 studio motel rooms is appropriate for the site location and size. Hours of operation, noise, and management of rubbish and recycling will be actively managed onsite. As such, adverse effects from the visitor accommodation operation on owners and occupiers of adjoining sites, and other persons, are considered to be less than minor.

4.3.3 Decision: Effects on Persons (s95E(1))

In terms of section 95E of the RMA, and on the basis of the above assessment, no person is considered to be adversely affected.

Therefore, limited notification is not required under Step 3.

4.4 Step 4 – Further Limited Notification in Special Circumstances (s95B(10))

Special circumstances do not apply that require limited notification.

5. OVERALL NOTIFICATION DETERMINATION

In reliance on the assessment undertaken in sections 3 and 4 above, the application is to be processed on a non-notified basis.

6. S104 ASSESSMENT

6.1 EFFECTS ON THE ENVIRONMENT (s104(1)(a))

Actual and potential effects on the environment have been outlined in section 3 and 4 of this report. Conditions of consent can be imposed under s108 of the RMA as required to avoid, remedy or mitigate adverse effects (s104)(1)(a).

6.2 RELEVANT DISTRICT PLAN PROVISIONS (s104(1)(b)(vi))

Operative District Plan

The relevant operative objectives and policies are contained within Parts 4 (District Wide), 7 (Residential Areas), 14 (Transport) and 22 (Earthworks) of the ODP.

The assessment against Parts 7 and 14 provided at section 6 of the applicant's AEE, is comprehensive and is considered accurate. It is therefore adopted for the purposes of this report, with the following additions:

Part 4 Objective 2 - Existing Urban Areas and Communities

Urban growth which has regard for the built character and amenity values of the existing urban areas and enables people and communities to provide for their social, cultural and economic well being.

The development will take place in a manner, form and location which protects or enhances the built character and amenity of Wanaka.

Part 14 Objective 5 - Parking and Loading - General Sufficient accessible parking and loading facilities to cater for the anticipated demands of activities while controlling adverse effects.

The proposed car parking is available, convenient and accessible to users including people with disabilities. The parking areas is designed and will be landscaped to mitigate any adverse visual effect on neighbours, including outlook and privacy, and will the safety of pedestrians as well as vehicles. The applicant proposes to actively manage car parking by allocating spaces to a specific guest to address the parking shortfall, and the site is located close to the Dungarvon Street bus stop and active transport routes.

Part 22 Objective 1 Enable earthworks that are part of subdivision, development, or access, provided that they are undertaken in a way that avoids, remedies or mitigates adverse effects on communities and the natural environment.

The proposed earthworks will provide a safe and stable building site and access with suitable gradients. Site management measures will avoid, remedy or mitigate adverse effects of earthworks.

Part 22 Objective 3 Ensure earthworks do not adversely affect the stability of land, adjoining sites or exacerbate flooding.

The proposed earthworks, in particular, cut and retaining, can be undertaken without adversely affect the stability of adjoining sites.

Overall, the proposal is considered to be consistent with, and not contrary to, the relevant objectives and policies of the ODP.

Proposed District Plan

The relevant operative objectives and policies are contained within Chapters 3 (Strategic Direction), 4 (Urban Development), 8 (Medium Density Residential), 25 (Earthworks) and 29 (Transport).

The assessment provided at section 6 of the applicant's AEE, is comprehensive and is considered accurate. It is therefore adopted for the purposes of this report, with the following additions:

4.2.2A Objective - A compact and integrated urban form within the Urban Growth Boundaries that is coordinated with the efficient provision and operation of infrastructure and services.

It is considered that the proposal promotes efficient use of urban zoned land. The development can be accommodated by existing infrastructure.

8.2.11 Objective - Visitor accommodation, residential visitor accommodation and homestays are enabled at locations, and at a scale, intensity and frequency, that maintain the residential character and amenity values of the zone.

In this instance, the proposed motel is considered of an appropriate scale for the surrounding medium density residential environment, and is appropriately located near the town centre and other amenities. The proposal will ensure that adverse effects on residential amenity values are avoided, remedied or mitigated, particularly in regard to active onsite management. Overall, the proposed visitor accommodation activity is considered not contrary to the objectives and policies within Chapter 8.

25.2.1 Objective – Earthworks are undertaken in a manner that minimises adverse effects on the environment, protects people and communities, and maintains landscape and visual amenity values.

The proposed development will be undertaken in accordance with required standards and conditions to ensure that land stability and road safety is managed during construction. The earthworks can be undertaken in a manner that will not adversely affect infrastructure, buildings and the stability of adjoining sites.

29.2.2 Objective – Parking, loading, access, and onsite manoeuvring that are consistent with the character, scale, intensity, and location of the zone

The site is well located for alternative transport modes. The parking shortfall will result in minor adverse effects on the function of the surrounding transport network and amenity of the surrounding environment. The development provides access and on-site manoeuvring that is safe and compatible with the classification of the adjoining local roads. The site layout and design will mitigate adverse effects on the amenity of the streetscape and adjoining sites.

Overall, the proposal is considered to be consistent with, and not contrary to, the relevant objectives and policies of the PDP.

Weighting between Operative District Plan and Proposed District Plan

In this case, as the conclusions reached in the above assessment lead to the same conclusion under both the ODP and PDP, no weighting assessment is required. In addition, given the application is frozen pursuant to s88A(1A), weight is given to the ODP under which the application is considered to be appropriate as the application is consistent with those relevant Objectives and Policies.

6.3 PARTICULAR RESTRICTIONS FOR NON-COMPLYING ACTIVITIES (s104(D))

With respect to the assessment above, the first gateway test for a non-complying activity required under section 104D(1)(a) has been met in that the application will not have an adverse effect on the environment which is more than minor.

With respect to the second gateway test under section 104D(1)(b), the application is not contrary to the relevant policies and objectives of the Operative District Plan or the Proposed District Plan.

Accordingly, as the application has passed both of the gateway tests in s104D, consent can be granted for this non-complying activity.

6.4 PART 2 OF THE RMA

Section 5(2) of the Resource Management Act 1991 details the purpose of the Act in promoting the sustainable management of the natural and physical resources. It is considered that the proposed development and visitor accommodation activity is appropriate in location, design and scale to maintain the amenity values of the site and surrounding area. The proposal includes mitigation measures to ensure that adverse effects are no more than minor. The proposal will enable the consent holder to provide for their social, economic, and cultural well-being, while ensuring the health and safety of guests and the community. The life supporting capacity of air, water, soil and ecosystems of the surrounding area is not significantly affected by the proposed activity as demonstrated in the above assessment.

The relevant matters listed in section 7 of the Act must also be given consideration. These matters include:

- (b) Efficient use of natural and physical resources*
- (c) The maintenance and enhancement of amenity values*
- (f) Maintenance and enhancement of the quality of the environment*

Amenity values are those natural and physical qualities and characteristics of an area, which contribute to people's appreciation of its pleasantness, aesthetic coherence, and cultural and recreational attributes. The definition of "Environment" is defined in the Act as follows:

- (a) Ecosystems and their constituent parts including people and communities: and*
- (b) All natural and physical resources: and*
- (c) The social, economic, aesthetic, and cultural conditions which affect the matters stated paragraphs (a) to (c) of this definition or which are affected by those matters.*

With regard to the matters raised in section 7 of the Act, it is considered that, as outlined within this report, the proposal will maintain existing amenity values and the quality of the existing environment.

Overall, the proposal is considered to meet the purpose and principles of the RMA.

6.5 DECISION ON RESOURCE CONSENT PURSUANT TO SECTION 104 OF THE RMA

Consent is **granted** to construct and operate 21 studio visitor accommodation units, with associated site and zone standard breaches, subject to the conditions outlined in *Appendix 1* of this decision report imposed pursuant to Section 108 of the RMA.

7.0 DEVELOPMENT CONTRIBUTIONS AND ADMINISTRATIVE MATTERS

Local Government Act 2002: Development Contributions

In granting this resource consent, pursuant to the Local Government Act 2002 and the Council's Policy on Development Contributions the Council has identified that a Development Contribution is required.

Please contact the Council if you require a Development Contribution Estimate.

Payment will be due prior to commencement of the consent, except where a Building Consent is required when payment shall be due prior to the issue of the code of compliance certificate.

Administrative Matters

The costs of processing the application are currently being assessed and you will be advised under separate cover whether further costs have been incurred.

The Council will contact you in due course to arrange the required monitoring. It is suggested that you contact the Council if you intend to delay implementation of this consent or if all conditions have been met.

This resource consent is not a building consent granted under the Building Act 2004. A building consent must be obtained before construction can begin.

This resource consent must be exercised within five years from the date of this decision subject to the provisions of section 125 of the RMA.

If you have any enquiries please contact Rosalind Devlin by email roz.devlin@qldc.govt.nz.

Report prepared by

Decision made by



Rosalind Devlin
CONSULTANT PLANNER

Sarah Gathercole
SENIOR PLANNER

- APPENDIX 1 – Consent Conditions**
- APPENDIX 2 – Applicant's AEE**
- APPENDIX 3 – Council Engineering Report**
- APPENDIX 4 – Council Urban Design Report**
- APPENDIX 5 – Council Acoustic Report**

APPENDIX 1 – CONSENT CONDITIONS

General Conditions

1. That the development must be undertaken/carried out in accordance with the plans:

Walker Architecture:

- 'Motel Concept Plan: Level 1 Floor A1.01 Rev F' dated 03.07.2019
- 'Motel Concept Plan: Level 2 Floor A1.02 Rev B' dated 03.07.2019
- 'Motel Concept Plan: Roof A1.03 Rev B' dated 03.07.2019
- 'Motel Concept Plan: Site Works A1.04' dated 03.07.2019
- 'Motel Concept Elevation A1.05.01 Rev G' dated 03.07.2019
- 'Motel Concept Sections A+B A1.06.01 Rev F' dated 03.07.2019
- 'Motel Concept Cross Section C A1.06.02 Rev E' dated 03.07.2019

Rough & Milne:

- 'Landscape Concept, Drawing L 1.0 Rev D' dated 23/07/2019
- 'Landscape Elevation, Drawing L 2.0 Rev D' dated 23/07/2019

stamped as approved on 17 November 2020

and the application as submitted, with the exception of the amendments required by the following conditions of consent.

2. This consent shall not be exercised and no work or activity associated with it may be commenced or continued until the following charges have been paid in full: all charges fixed in accordance with section 36(1) of the Resource Management Act 1991 and any finalised, additional charges under section 36(3) of the Act.
3. The consent holder is liable for costs associated with the monitoring of this resource consent under Section 35 of the Resource Management Act 1991.
4. Prior to any works commencing on site the Consent Holder shall complete the Short Form Environmental Management proforma:

<https://www.qldc.govt.nz/planning/resource-consents/guidelines-for-environmental-management-plans/>

At all times during the works, environmental management measures onsite shall be installed and carried out in accordance with this document.

5. Prior to commencing ground-disturbing activities, the Consent Holder shall nominate an Environmental Representative for the works program in accordance with the requirements detailed on pages 9 and 10 of the *Queenstown Lakes District Council's Guidelines for Environmental Management Plans*.
6. Prior to commencing ground disturbing activities, the Consent Holder shall ensure that all staff (including all sub-contractors) involved in, or supervising, works onsite have attended an Environmental Site Induction in accordance with the requirements detailed on page 8 of the *Queenstown Lakes District Council's Guidelines for Environmental Management Plans*.
7. The EMP shall be accessible on site at all times during work under this consent.
8. In accordance with page 9 of the *Queenstown Lakes District Council's Guidelines for Environmental Management Plans*, where any Environmental Incident where the EMP has failed leading to any adverse environmental effects offsite occurs the Consent Holder shall report to QLDC details of any Environmental Incident within 12 hours of becoming aware of the incident.

Design Controls

9. The 'fins' shall be reduced in height by 30%.
10. All visitor accommodation unit doors and the reception door fronting the carpark area shall have glazed vision panels.
11. All air conditioning plants and vents shall be screened from direct view from public roads. Should any plant system need to be installed on the roof, the unit shall not be directly visible from street level.
12. Prior to commencement of the visitor accommodation activity, a lighting plan shall be submitted to Council's Monitoring Officer for certification. All exterior lighting is to comply with the Southern Lights Strategy. Lighting within the carpark area shall incorporate capping in their design so as to be downlit only to reduce light spill upwards. Council may appoint an urban design specialist to certify the report.

Landscaping

13. The landscape plans approved as part of this consent shall be implemented within the first planting season following construction. All planting shall be maintained as per the approved landscape plans (includes planting, management and maintenance). If any plant shall die or become diseased it shall be replaced within 6 months, or the next available planting season, as per the approved landscape plan. Any amendment to the specified plant species shall be certified by the Council Planning and Development division prior to implementation.
14. Fencing along the Dungarvon and Brownston Street road boundaries shall be relocated to the inside of the *Sophora Microphylla* plantings, and shall be either:

- (i) a minimum of 50% visually permeable if over 1.4m high; or
- (ii) if no higher than 1.2m, fencing may be solid.

This condition does not apply to internal site boundaries.

15. The tree species *Pseudopanax crassifolius* shall have a planting size grade of no less than 10l and the *Sophora Microphylla* shall be no less than 25l. The *Sophora Microphylla* shall be crown lifted to 2m above ground once they have reached a height of 4m and all areas of mass planting shall be managed to achieve a closed canopy no more than 1m in height.
16. A step free, firm, slip resistant pathway is to be installed from the accessible car parking space to the main building and common areas of the building providing a minimum clear width of 1200mm.
17. At least one step free, firm, slip resistant paths is to be installed where links to the public footpaths on Dungarvon and Brownston Streets have been shown on the approved plans. The footpath across the grassed berm areas shall be exposed aggregate.

Operational Conditions

18. Prior to the use of the site for visitor accommodation, the consent holder shall submit a site management plan to the Council's Monitoring Department for certification. The approved site management plan must be implemented in perpetuity for the visitor accommodation activity on the site.

The objective of the site management plan is to outline the management techniques that will be used to ensure conditions (19-31) are met, and shall include the contact details of the property manager available for any complaints.

19. Each visitor accommodation unit shall be rented to a maximum of one (1) group at any one time and the maximum number of guests for each unit shall be restricted to 2 adults and 2 children at any one time.

20. The consent holder shall be responsible for ensuring that all rubbish and recycling shall be disposed of appropriately. Where there is kerbside collection used, rubbish and recycling shall only be placed on the street the day of or the day prior to collection.
21. A 2m high solid (acoustic) fence/wall with no gaps shall be constructed on the boundary between the driveway/car park/service area and 37 Dungarvon Street/90 Brownston Street.
22. Prior to commencement of construction, the consent holder shall submit a Construction Noise and Vibration Management Plan to Council's Monitoring Officer for certification. Council may appoint an acoustics specialist to certify the report.
23. Construction shall be undertaken in accordance with the Construction Noise and Vibration Management Plan approved by condition (21) and in compliance with NZS 6803.
24. All sound from building services equipment shall comply with a noise limit of 40 dB LAeq. Prior to prior to occupation of the building, this noise compliance shall be verified by an acoustics specialist, and a report submitted to Council's Monitoring Officer for certification. Council may appoint an acoustics specialist to certify the report.
25. Visitor accommodation activities shall comply with the relevant District Plan noise limits at all times.
26. Service vehicles/activity shall occur only between 0800h and 2000h.
27. Loudspeakers or music shall not be played outside the visitor accommodation building at any time.
28. Visitor accommodation doors and windows must be kept closed between 2000h and 0800h, and at all times music is played inside, except when guests enter/exit the units.
29. Visitor accommodation guests shall be informed of, and sign an agreement, to comply with noise restrictions prior to occupation. Signage requiring noise minimisation shall be displayed in each visitor accommodation unit.

Parking

30. Where a visitor is accessing the visitor accommodation facility by private vehicle, the visitor accommodation operator shall manage car parking by allocating spaces to a specific guest at the time of booking. If there is no car parking on site available when a potential guest requests accommodation, the operator will inform the potential guest that there are no on-site spaces available, and inform them of other local parking options. Guests shall not park on adjacent residential streets.
31. The visitor accommodation operator shall ensure that no coaches are to service the proposed activity.

Engineering

General

32. All engineering works, including the construction of retaining walls, shall be carried out in accordance with the Queenstown Lakes District Council's policies and standards, being QLDC's Land Development and Subdivision Code of Practice adopted on 3rd May 2018 and subsequent amendments to that document up to the date of issue of any resource consent.

Note: The current standards are available on Council's website via the following link:

<http://www.qldc.govt.nz>

To be completed prior to the commencement of any works on-site

33. The consent holder shall obtain and implement a traffic management plan approved by Council prior to undertaking any works within or adjacent to Council's road reserve that affects the normal

operating conditions of the road reserve through disruption, inconvenience or delay. The Traffic Management Plan shall be prepared by a Site Traffic Management Supervisor (STMS). All contractors obligated to implement temporary traffic management plans shall employ a qualified STMS to manage the site in accordance with the requirements of the NZTA's *Traffic Control Devices Manual Part 8: Code of practice for temporary traffic management*. The STMS shall implement the Traffic Management Plan. A copy of the approved plan shall be submitted to the Manager of Resource Management Engineering at Council prior to works commencing.

34. The owner of the land being developed shall provide a letter to the Manager of Resource Management Engineering at Council advising who their representative is for the design and execution of the engineering works and construction works required in association with this development and shall confirm that these representatives will be responsible for all aspects of the works covered under Sections 1.7 & 1.8 of QLDC's Land Development and Subdivision Code of Practice, in relation to this development.
35. Prior to commencing works on the site, the consent holder shall obtain 'Engineering Review and Acceptance' from the Queenstown Lakes District Council for development works to be undertaken and information requirements specified below. The application shall include all development items listed below unless a 'partial' review approach has been approved in writing by the Manager of Resource Management Engineering at Council. The 'Engineering Review and Acceptance' application(s) shall be submitted to the Manager of Resource Management Engineering at Council for review, prior to acceptance being issued. At Council's discretion, specific designs may be subject to a Peer Review, organised by the Council at the applicant's cost. The 'Engineering Review and Acceptance' application(s) shall include copies of all specifications, calculations, design plans and Schedule 1A design certificates as is considered by Council to be both necessary and adequate, in accordance with Condition (31), to detail the following requirements:
- a) The provision of a water supply to the development. This shall include an approved valve and valve box with backflow prevention and provision for water metering to be located at the road reserve boundary. The backflow prevention device shall be located on the subject site. The existing water connection shall be removed and capped at the main. The costs of the connection shall be borne by the consent holder.
 - b) The provision of a foul sewer connection to the development. In the event the existing wastewater connection becomes redundant it shall be removed and capped at the main. The costs of the connection shall be borne by the consent holder.
 - c) The provision of a stormwater collection and disposal system which shall provide both primary and secondary protection for future development within the development, in accordance with Council's standards and connection policy. This shall include:
 - (i) The provision of soak pits/infiltration galleries adequate to dispose of the runoff from the development during the critical 5% AEP storm event. Percolation testing shall be undertaken by a suitably qualified professional at the individual soak pit/infiltration gallery locations to adequately demonstrate that soakage is available in all areas proposed for soakage. The method for soakage testing is to be agreed with the Manager of Resource Management Engineering at Council prior to testing. A copy of the test results shall be provided to the Manager of Resource Management Engineering at Council along with the design of the soak pits/infiltration galleries based on the percolation testing results. The soak pit/infiltration gallery design shall be in general accordance with the "Acceptable Solutions and Verification Methods for New Zealand Building Code Clause: E1/VM1 Surface Water," except for using the 5% AEP storm event, taking into account the critical storm duration and ensuring the soak pits/infiltration galleries will drain within 24 hours of the end of the critical event.
 - (ii) A secondary protection system consisting of secondary flow paths to cater for the 1% AEP storm event and/or setting of appropriate building floor levels to ensure that there is no inundation of any building, and no increase in the effects due to run-off onto land beyond the site from the pre-development situation.

- d) The provision of PS1 Producer Statement for design from a suitably qualified Chartered Professional Engineer for the retaining wall near the southeast and southwest boundaries. This shall include plans demonstrating that the retaining wall and any required drainage are located wholly within the site.
 - e) The provision of PS1 Producer Statement for design from a suitably qualified Chartered Professional Engineer for the retaining wall near the Dungarvon Street road reserve boundary, confirming that the wall has been designed to withstand a suitable surcharge load from vehicular traffic. This shall include plans demonstrating that the retaining wall and associated drainage are located wholly within the site.
 - f) The provision of an approved temporary 'Licence to Occupy' from the Council for all temporary works and occupations (i.e. temporary batter slopes and hoarding) required in Council's road reserve during the motel's construction.
 - g) The provision of a sealed vehicle crossing that shall be constructed to the development to Council's standards.
 - h) The removal of the existing vehicle crossing to the site. This shall include:
 - (i) Reinstatement of grassed berm between the footpath and the property boundary.
 - (ii) Removal of the existing dropped kerb and provision of kerbing commensurate with the surrounds.
 - (iii) Regrading of the footpath at the location of the existing vehicle crossing.
 - i) The construction and sealing of all vehicle manoeuvring and car parking areas to Council's standards. This shall include:
 - (i) Parking and loading spaces shall be clearly and permanently marked out.
 - (ii) The area required for turning at the north end of the parking area shall be clearly and permanently marked as a no parking area.
 - (iii) All signage and markings are to be in compliance with the NZTA Manual of Traffic Signs and Markings.
 - (iv) Provision shall be made for stormwater disposal.
 - j) The provision of Design Certificates for all engineering works associated with this development submitted by a suitably qualified design professional (for clarification this shall include all Roads, Water, Wastewater and Stormwater reticulation). The certificates shall be in the format of the QLDC's Land Development and Subdivision Code of Practice Schedule 1A Certificate.
36. Prior to commencing any work on the site the consent holder shall install measures to control and/or mitigate any dust, silt run-off and sedimentation that may occur, in accordance with QLDC's Land Development and Subdivision Code of Practice to ensure that neighbouring sites remain unaffected from earthworks. These measures shall be implemented **prior** to the commencement of any earthworks on site and shall remain in place for the duration of the project, until all exposed areas of earth are permanently stabilised.
37. At least 7 days prior to commencing excavations, the consent holder shall provide the Manager of Resource Management Engineering at Council with the name of a suitably qualified geo-professional as defined in Section 1.7 of QLDC's Land Development and Subdivision Code of Practice who shall supervise the excavation procedures and retaining wall construction. This engineer shall continually assess the condition of the excavation and shall be responsible for ensuring that temporary retaining is installed wherever necessary to avoid any potential erosion or instability.

To be monitored throughout earthworks

38. No permanent batter slope within the site shall be formed at a gradient that exceeds 1(V):2(H).

39. The excavation and retaining wall construction near the southeast and southwest boundaries shall be undertaken in accordance with the recommendations of Geotek Consulting (email dated 8 September 2019 from Steven Roberts to Cam Jones 'Re: Fw: Wanaka Motel Geotech (2002-17).')
40. Temporary retention systems shall be installed wherever necessary immediately following excavation to avoid any possible erosion or instability.
41. The consent holder shall implement suitable measures to prevent deposition of any debris on surrounding roads by vehicles moving to and from the site. In the event that any material is deposited on any roads, the consent holder shall take immediate action, at his/her expense, to clean the roads. The loading and stockpiling of earth and other materials shall be confined to the subject site.
42. No earthworks, temporary or permanent, are to breach the boundaries of the site, except those required for the removal of the existing vehicle crossing, provision of a new vehicle crossing and services, and as permitted by an approved Licence to Occupy.

To be completed before occupation of the building

43. Prior to the occupation of the building, the consent holder shall complete the following:
 - a) The submission of 'as-built' plans and information required to detail all engineering works completed in relation to or in association with this development at the consent holder's cost. This information shall be formatted in accordance with Council's 'as-built' standards and shall include all Water, Wastewater and Stormwater reticulation (including private laterals and toby positions).
 - b) The completion and implementation of all reviewed and accepted works detailed in Condition (35) above.
 - c) An Elster Helix 4000 or C4000 / 4200 or Sensus Meitwin; Meistream; WP water meter shall be installed on to the Acuflo manifold as per Condition (35a) above.
 - d) The provision of PS4 Producer Statement for design from a suitably qualified Chartered Professional Engineer for the retaining wall near the southeast and southwest boundaries. This shall include plans demonstrating that the retaining wall and any required drainage are located wholly within the site.
 - e) All earthworked areas shall be top-soiled and revegetated or otherwise permanently stabilised.
 - f) The consent holder shall remedy any damage to all existing road surfaces and berms that result from work carried out for this consent.
 - g) The submission of Completion Certificates from both the Contractor and Accepted Engineer for all infrastructure engineering works completed in relation to or in association with this development (for clarification this shall include all Roads, Water, Wastewater and Stormwater reticulation). The certificates shall be in the format of the QLDC's Land Development and Subdivision Code of Practice Schedule 1B and 1C Certificate.

Ongoing Conditions/Covenants

44. In the event that the Engineering Acceptance issued under Condition (35) contains ongoing conditions or requirements associated with the installation, ownership, monitoring and/or maintenance of any infrastructure subject to Engineering Acceptance, then at Council's discretion, a Covenant in Gross (or other alternative legal instrument acceptable to Council) shall be registered on the relevant Records of Title detailing these requirements for the lot owner(s). The final form and wording of the document shall be checked and approved by Council's solicitors at the consent holder's expense prior to registration to ensure that all of the Council's interests and liabilities are adequately protected. The applicant shall liaise with the Subdivision Planner and/or Manager of

Resource Management Engineering at Council in respect of the above. All costs, including costs that relate to the checking of the legal instrument by Council's solicitors and registration of the document, shall be borne by the applicant.

[Note: This condition is intended to provide for the imposition of a legal instrument for the performance of any ongoing requirements associated with the ownership, monitoring and maintenance of any infrastructure within this development that have arisen through the detailed engineering design and acceptance process, to avoid the need for a consent variation pursuant to s.127 of the Resource Management Act].

45. On completion of earthworks, a covenant pursuant to Section 108(2)(d) of the Resource Management Act 1991 shall be registered on the Register of Title of the subject site providing for the performance of any ongoing requirements for protection of secondary flow paths or minimum floor levels for buildings, where deemed necessary by Council to satisfy Condition 35(c)(ii) above. The final wording of the instrument shall be checked and approved by the Council's solicitors at the consent holder's expense prior to registration to ensure that all of the Council's interests and liabilities are adequately protected.

Review

46. At any time, within ten working days the Council may, in accordance with Sections 128 and 129 of the Resource Management Act 1991, serve notice on the consent holder of its intention to review the conditions of this resource consent for any of the following purposes:
- a) To deal with any adverse effects on the environment that may arise from the exercise of the consent which were not foreseen at the time the application was considered and which it is appropriate to deal with at a later stage.
 - b) To deal with any adverse effects on the environment which may arise from the exercise of the consent and which could not be properly assessed at the time the application was considered.
 - c) To avoid, remedy and mitigate any adverse effects on the environment which may arise from the exercise of the consent and which have been caused by a change in circumstances or which may be more appropriately addressed as a result of a change in circumstances, such that the conditions of this resource consent are no longer appropriate in terms of the purpose of the Resource Management Act 1991.
 - d) The purpose of this review is in relation to effects on any person in relation to nuisance (including but not limited to noise, rubbish/recycling, and car parking).

Advice Notes

1. This consent triggers a requirement for Development Contributions, please see the attached information sheet for more details on when a development contribution is triggered and when it is payable. For further information, please contact the DCN Officer at QLDC.
2. No signs have been approved by this resource consent.

For Your Information

If your decision requires monitoring, we will be sending an invoice in due course for the deposit referred to in your consent condition. To assist with compliance of your resource consent and to avoid your monitoring deposit being used before your development starts, please complete the "[Notice of Works Starting Form](#)" and email to the Monitoring Planner at RCMonitoring@qldc.govt.nz prior to works commencing.

You may also have conditions that require you to apply for Engineering Acceptance. To apply for Engineering Acceptance, please complete the [Engineering Acceptance Application form](#) and submit this completed form and an electronic set of documents to engineeringacceptance@qldc.govt.nz with our monitoring planner added to the email at RCMonitoring@qldc.govt.nz.

If your decision requires a development contribution (DC) charge, we will be sending a notice in due course. To answer questions such as what is a DC charge, when a DC charge is triggered and timing of payments, please refer to this link. <http://www.qldc.govt.nz/planning/development-contributions/> If you wish to make a DC estimate calculation yourself, please use this link: <http://www.qldc.govt.nz/planning/development-contributions/development-contributions-estimate-calculator/> And for full details on current and past policies, please use this link: <http://www.qldc.govt.nz/council-online/council-documents/policies/policy-on-development-contributions-and-financial-contributions/>

APPENDIX 2 – APPLICANT’S AEE



TOWNPLANNING
GROUP

Application for Resource Consent to the Queenstown Lakes District Council:

C & J Limited

*To establish and operate a motel at 86
Brownston Street, Wanaka*

29 January 2018



Document prepared by:

Town Planning Group Limited

Phone: 0800 22 44 70

Email: office@townplanning.co.nz

Web: www.townplanning.co.nz

Offices in Queenstown & Christchurch



Application Contents

1.	Introduction.....	3
2.	Site & Surrounds	5
3.	Proposal Details.....	7
	Proposal Overview	7
4.	Statutory Provisions	9
	Queenstown Lakes Operative District Plan.....	9
	Queenstown Lakes Proposed District Plan.....	15
	National Environmental Standards	16
5.	Assessment of Effects on the Environment.....	17
	Visitor Accommodation Activity	17
	Road Setback.....	18
	Boundary Planting	20
	Building Height.....	20
	Recession planes	21
	Parking	23
	Positive effects.....	24
	Conclusion	24
6.	Statutory Assessment.....	25
	Objectives and Policies	25
	Section 95 of the RMA.....	0
	Purpose & Principles of the Resource Management Act	0

Supporting Information

- A Computer Freehold Register
- B Development Plans
- C Landscape Plan

1. Introduction

- 1.1 C & J Limited (“**the Applicant**”) applies for resource consent to establish a two-storey motel complex comprising 21 guest accommodation units, at 86 Brownston Street, Wanaka (“**the site**”).
- 1.2 The site is approximately 1,002m² with frontage to both Brownston Street and Dungarvon Street.
- 1.3 The site is zoned **Low Density Residential** under the operative Queenstown Lakes District Plan (“**Operative District Plan**”). Resource consent is required for:
- Discretionary activity, Rule 7.5.3.4 (i), Visitor accommodation activity in the low density residential zone.
 - Restricted Discretionary Activity, Rule 7.5.3.4 (vi), breach of site standards 7.5.5.2(iii) setback from roads and 7.5.5.2 (xiii) boundary planting.
 - Non-complying Activity, Rule 7.5.3.5, breach of zone standard 7.5.5.3(v) relating to building height and recession lines.
 - Restricted Discretionary activity, Rule 14.2.2.3 (ii), breach of site standards; 14.2.4.1(i) minimum parking space numbers, and 14.2.4.1 (v) size of parking spaces.
- 1.4 Under the Queenstown Lakes Proposed District Plan (“**Proposed District Plan**”), the site is zoned Medium Density Residential. Notable changes from the operative plan zone rules include: the reduction of the road setback from 4 metres to 3 metres; removal of recession planes along the road boundary; less intrusive recession place on other boundaries; and reduced parking requirements. This proposal has been designed to take into account the scale and nature of development anticipated by the proposed district plan with regard also been given to the operative plan provisions.
- 1.5 Overall consent is required for a **Non-Complying Activity** under the operative District Plan
- 1.6 In summary, this Assessment of Environmental Effects report determines that the proposal will have effects that are less than minor within the receiving environment. The proposal as a whole is not contrary to relevant objectives and policies of the

operative and proposed District Plans and promotes the principle of sustainable management of natural and physical resources, in accordance with Part 2 of the Resource Management Act 1991 (“**the RMA**”).

2. Site & Surrounds

- 2.1 The application site is legally described as Section 1 Block XXIII Town of Wanaka in Computer Freehold Register OT332/41 (**Attachment A**). There are no encumbrances on the title that affect this application.
- 2.2 The site and the surrounding environment is identified in Figures 1 and 2 below.

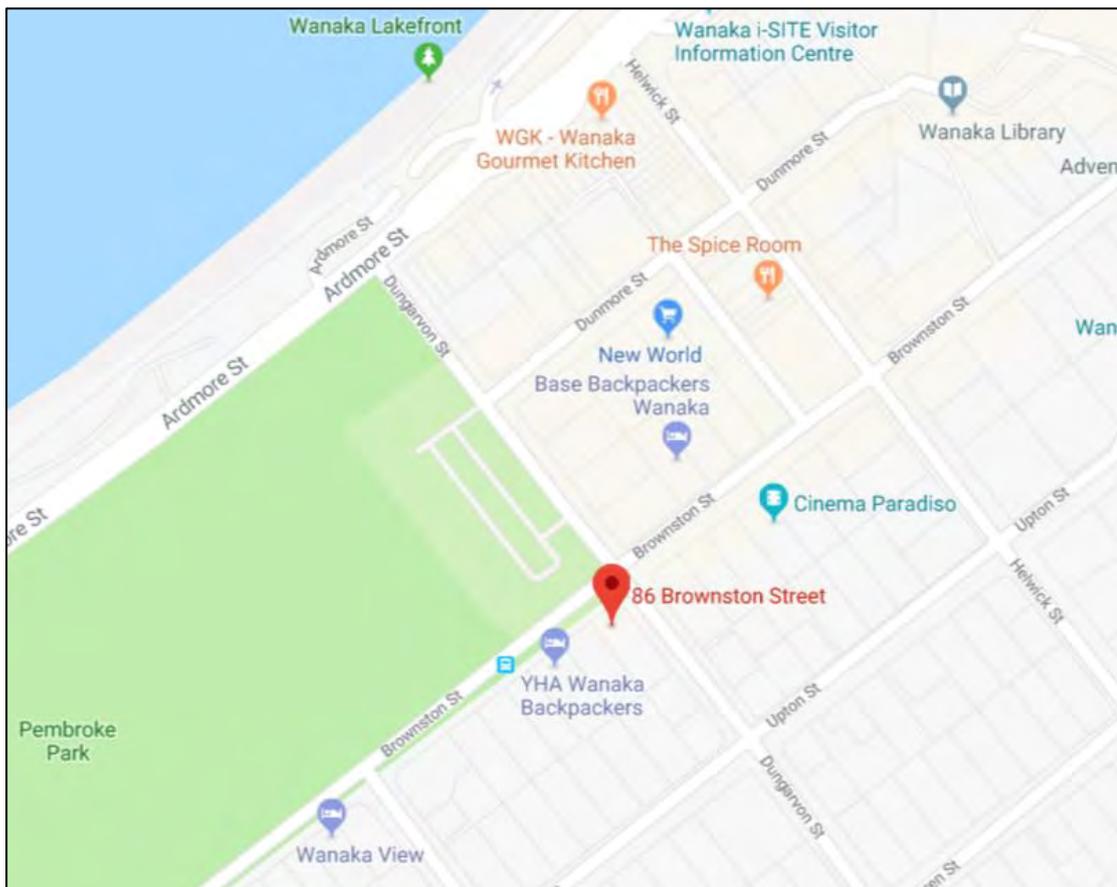


Figure 1: Overview of Site (Source: Google Maps)

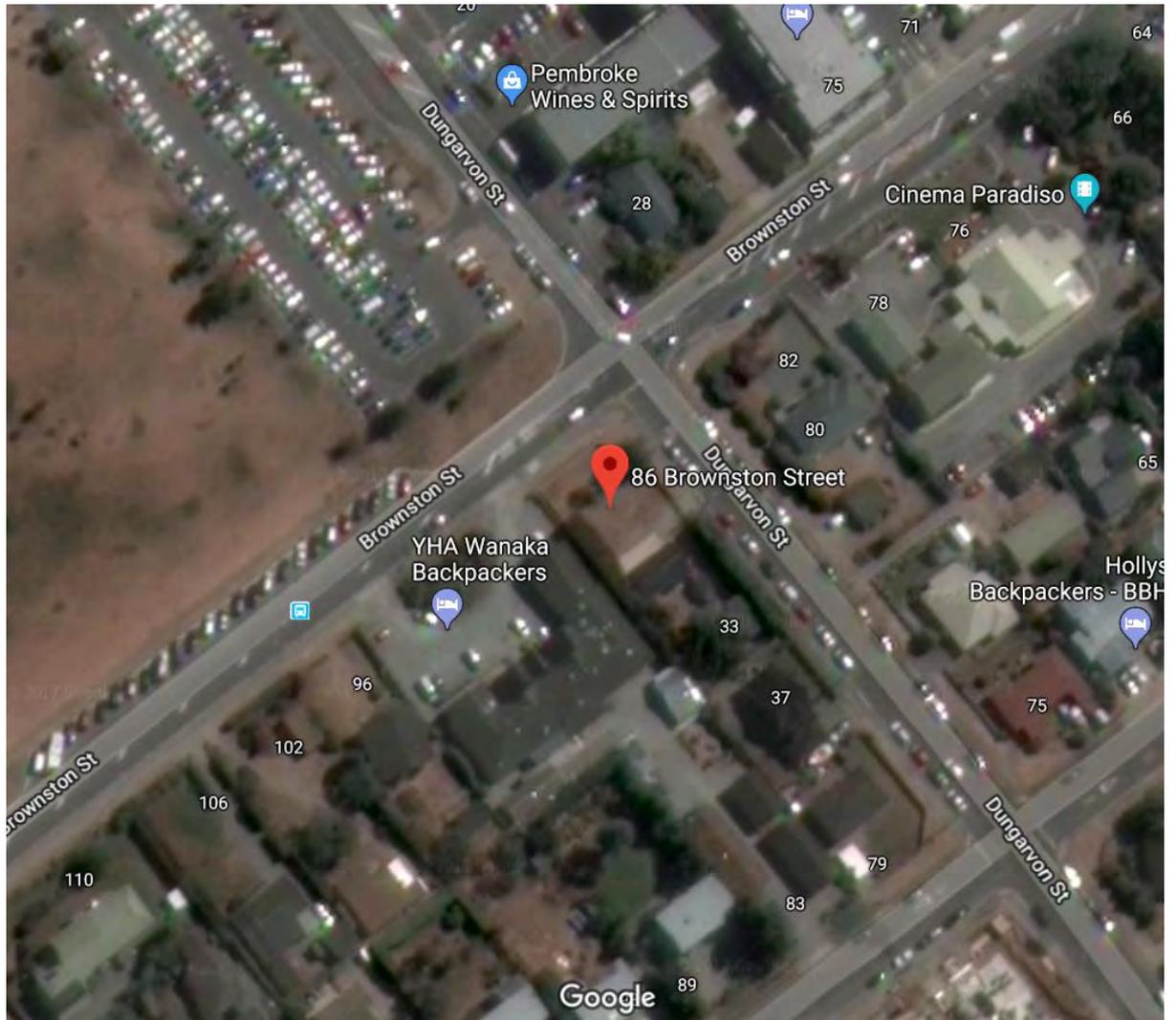


Figure 2: Aerial view of Site (Source: Google Maps)

- 2.3 A single residential dwelling is currently located on the site, with the environment to the south and east generally characterised by similar residential development with visitor accommodation activity interspersed. The Wanaka Youth Hostel is located immediately to the west of the site, with this also extending through to Upton Street to the south. A public car park is located to the north, with the Wanaka Town Centre located immediately on the opposite side of the Brownston and Dungarvon intersection.
- 2.4 Brownston Road is classified as a Collector Road and Dungarvon Street is classified as a Local Road.

3. Proposal Details

Proposal Overview

- 3.1 The proposal is to establish a new two-storey motel building on the site, as identified in the Development Plans enclosed as **Attachment B**. The Applicant is advancing this proposal as a direct result to the significant demands on visitor accommodation in Wanaka. The site exhibits a number of positive characteristics to cater for the activity and built form; the proximity and easy access to the Wanaka Town Centre is a considerable positive factor in determining the site for the proposed activity, which is also reflected in similar land uses in the immediate locale.
- 3.2 A total of 21 guest units will be provided on the site. The ground floor will comprise eleven guest units over the building footprint area of 325m². On the first floor, there will be ten guest accommodation units, an office and a laundry. The gross floor area of the building will be approximately 618m². The site area is 1,002m², equating to a site coverage of approximately 32.5%.
- 3.3 The building will be a maximum of approximately 8.15m above ground level, while the lowest part of the roofline is approximately 6m above ground level. The undulating roof line provides an alpine appearance and creates visual interest to help mitigate the additional building height; this is discussed further in the AEE.
- 3.4 The length of building along the southwest boundary, bordering the neighbouring YHA Backpackers on Brownston Street is set back approximately 3m from the boundary, with the majority of the building stepped back to 9.5m from the boundary. The length of the building along the south-east boundary, adjoining the residence at 37 Dungarvon Street, is 8.6 metres.
- 3.5 The building design breaches a number of standards under the operative District Plan relating to setbacks from the road, building height and recession planes. The building is setback 2m from the road boundary, where the operative plan standard requires a setback of 4.5m. The proposed building height is 8.15 metres, exceeding the plan standard height of 7 metres. Recession plane breaches are shown on the development plans (**Attachment B**). In summary, recession planes are breached at the north west corner of the site, at approximately 4 metres above ground level,

adjacent to the YHA Backpackers at 90 Brownston Street. There is also a recession plane breach adjacent to the residential property at 37 Dungarvon Street, at approximately 5.5 metres above ground level.

- 3.6 The development will comprise a mixture of cedar and coloursteel cladding, with signage yet to be determined however it is anticipated that this will be directly affixed to the building at ground floor level.
- 3.7 Landscaping is proposed along both road frontages to help integrate the building into the setting and to soften the built form. Landscape plans for the development prepared by Rough & Milne Landscape Architects are enclosed as **Attachment C**. Approximately 200m² of landscape coverage is proposed, equating to 20% of the site coverage.
- 3.8 Fifteen car parks will be provided on site, inclusive of one mobility parking space. Access to the site is from Dungarvon Street.. There is an existing vehicle access to the property from Dungarvon Street, however this will be moved further away from the Dungarvon / Brownston intersection to the south-east corner of the property (shown on the development plans, **Attachment B**), as part of this development.
- 3.9 Exterior lighting will be designed to avoid windows on the adjoining sites, to minimise effects on neighbouring properties, and meet the plan standards. Exterior signage will also be designed to meet the Plan standards.
- 3.10 The units have been designed with a practical and accessible outdoor areas; the intent is that the motels will provide short term accommodation and that guests will spend the majority of their time enjoying Wanaka and the surrounds. Being a managed motel unit, the hours of operation will be flexible. The Applicant intends to comply with the noise standards under the operative District Plan and a consent condition to this effect is anticipated.

4. Statutory Provisions

Queenstown Lakes Operative District Plan

- 4.1 Under the operative District Plan, the site is zoned Low Density Residential, as shown in [Figure 3](#) below.

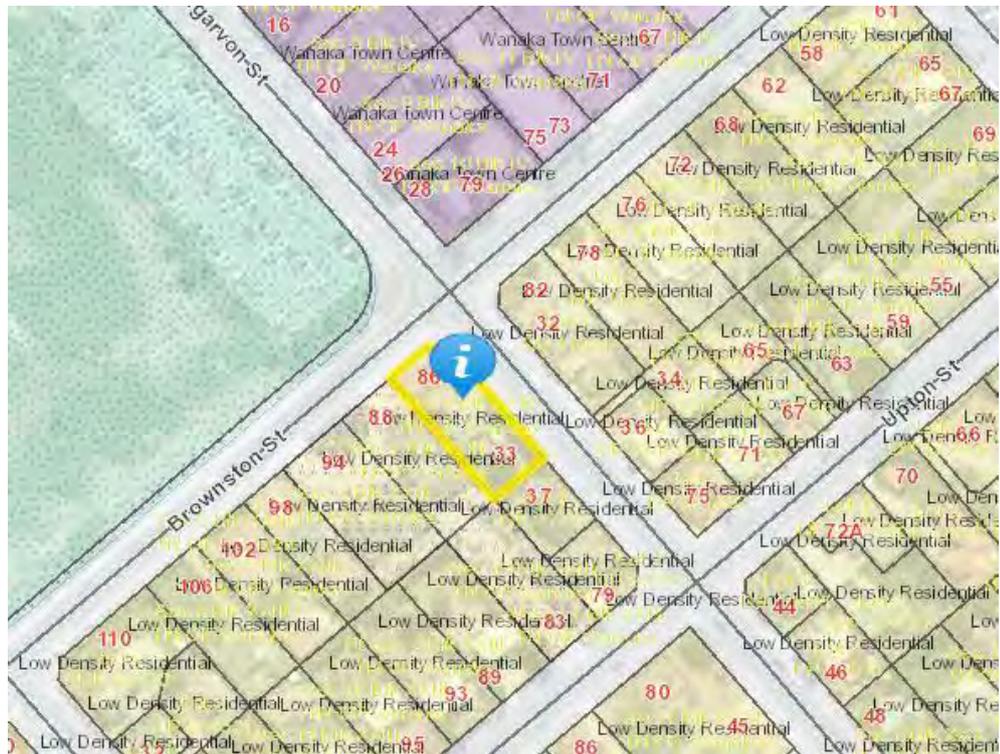


Figure 3: Project site with District Plan zoning (Source: QLDC Planning Maps).

- 4.2 A compliance assessment of the proposal against the relevant rules and standards of the operative District Plan is detailed in [Table 1](#) below.

Table 1: Operative District Plan Compliance Assessment

Rule Reference	Explanation / Requirement	Activity Status / Comment
Chapter 7 – Residential Areas		
7.5.3 Activities		
7.5.3.2 (iii)	Buildings for (a) non-residential activities; and... In respect of the matters listed in Assessment Matter 7.7.2 (iii)	Not applicable. Does not comply with all site and zone standards.
7.5.3.4 (i)	Visitor accommodation in the low density residential zone, excluding the visitor accommodation sub-zone, in respect of: (a) The location, external appearance and design of buildings; (b) The location, nature and scale of activities on site; (c) The location of parking and buses and access; (d) Noise, and (e) Hours of operation	Restricted Discretionary
7.5.3.4 (vi)	Any Activity which is not listed as a Non-Complying Activity or Prohibited Activity and which complies with all the Zone Standards but does not comply with one or more of the Site Standards shall be a Discretionary Activity with the exercise of the Council's discretion being confined to the matter(s) specified in the standard(s) not complied with.	Restricted Discretionary
7.5.3.5	Any activity which is not listed as a Prohibited Activity and which does not comply with one or more of the relevant Zone standards, shall be a Non Complying Activity.	Non-complying Does not comply with zone standard 7.5.5.3 (v) – Building height.
7.5.5.2 Site Standards		
7.5.5.2 (iii)	Setbacks from Roads (a) In the Low Density Residential Zone The minimum setback from road boundaries of any building, other than garages, shall be 4.5m.	Does not comply The building will be set back 2m from both Dungarvon Street and Brownston Street.
7.5.5.2 (iv)	Setbacks from Internal Boundaries / Neighbours (a) Except as provided for below, the minimum setback from internal boundaries/neighbours for any building shall be: Front Site: One setback of 4.5m and all other setbacks 2m.	Complies The building will be set back at least 4.5m from the southeast boundary, and at least 2m from the southwest boundary.
7.5.5.2 (v)	Access	Complies

	Each residential and/or visitor accommodation unit shall have legal access to a formed road.	
7.5.5.2 (vi)	<p>Continuous Building Length in the Low Density Residential Zone Where the aggregate length along one elevation of buildings measured parallel to any internal boundary or internal boundaries exceeds 16m; either:</p> <p>(a) The entire building(s) shall be set back an additional 0.5m for every 6m of additional length or part thereof from the minimum yard setback (continuous façades) at the same distances from the boundary; or</p> <p>(b) That part of the building(s) which exceeds the maximum building length shall be progressively set back 0.5m for every 6m of additional length or part thereof from the minimum yard setback (varied façade(s) with stepped setbacks from the boundary).</p>	<p>Complies</p> <p>The length of building along the southwest boundary is 41.7m. 8.6m of the building length is set back approximately 3m from the boundary. The remaining 33.1m is stepped back to 9.5 metres from the boundary.</p>
7.5.5.2 (ix)	<p>Non-reflective buildings All metal cladding, roofing or fences shall be painted or otherwise coated with a non-reflective finish.</p>	Complies
7.5.5.2 (xi)	<p>Flood Risk No building, greater than 20m² gross floor area, shall be constructed or relocated, with a ground floor level less than: (b) RL 281.9m above sea level (381.9m Otago Datum) at Wanaka.</p>	Complies
7.5.5.2 (xiii)	<p>Boundary Planting (Wanaka) No tree or hedgerow boundary planting shall exceed 1.9 metres in height within 2 metres of the boundary, at any point of its length.</p>	<p>Does not comply</p> <p>There is an existing hedge on the site which is not proposed to be removed. Trees exceeding 1.9 metres are proposed to be planted in front of the building within 2 metres of the boundary.</p>
7.5.5.3 Zone Standards		
7.5.5.3 (ii)	<p>Building Coverage The maximum building coverage for all activities on any site shall be: (a) 40% in the Low Density Residential Zone</p>	<p>Complies</p> <p>Building coverage is 326m², 33% of the 1,002m² site.</p>
7.5.5.3 (v)	Building height	Does not comply



	(a) Flat sites where the ground slope is equal to or less than 6 degrees (i.e. equal to or less than 1 in 9.5). The maximum height for buildings shall be 8.0m, and in addition no part of any building shall protrude through a recession line inclined towards the site at an angle of 25° and commencing at 2.5m above ground level at any given point on the site boundary: Except: (iii) The maximum height for buildings in the Residential Low and High Density Zones at Wanaka shall be 7m.	Building height is 8.15 metres at its highest point, and will breach the 25° recession line at 4 metres height, at the lowest point.
7.5.5.3 (vii)	Glare (a) All fixed exterior lighting shall be directed away from the adjacent sites and roads; and (b) No activity on any site shall result in greater than a 3.0 lux spill (horizontal and vertical) of light onto any other site measured at any point inside the boundary of the other site.	Complies Lighting will be designed to comply with this standard.
7.5.5.3 (xii)	Noise (a) Sound from visitor accommodation activities measured in accordance with NZS 6801:2008 and assessed in accordance with NZS 6802:2008 shall not exceed the following noise limits at any point within any other site in this zone: (i) daytime (0800 to 2000 hrs) 50 dB LAeq(15 min) (ii) night-time (2000 to 0800 hrs) 40 dB LAeq(15 min) (iii) night-time (2000 to 0800 hrs) 70 dB LAFmax (b) Sound from visitor accommodation activities which is received in another zone shall comply with the noise limits set in the zone standards for that zone.	Complies Noise will be managed to comply with this standard.
Chapter 14 – Transport		
14.2.2 Activities		
14.2.2.3 (ii)	Any activity which does not comply with the following Site Standards shall be a Discretionary Activity with the exercise of the Council's discretion being restricted to the matter(s) specified in that standard.	Restricted Discretionary Does not comply with site standards as below
14.2.4 Site Standards		
14.2.4.1 (i)	Minimum parking space numbers	Does not comply



	Activities shall provide on-site parking space in accordance with Table 1.	2 spaces per unit are required; a total requirement of 42 spaces. 15 spaces are provided.
14.2.4.1 (iv)	Location and availability of parking spaces	Complies Parking area will meet this standard.
14.2.4.1 (v)	Size of Parking Spaces	Does not comply Aisle width is 6m. For class 2 users, 2.7m stall width is required. 2.5m is provided. 8m aisle width is required for Disabled parks, only 6m is provided.
14.2.4.1 (vi)	Parking Area and Access Design	Complies
14.2.4.1 (vii)	Gradient of Car Parks Car parking areas shall have a gradient of no more than 1 in 20 in any one direction.	Complies The car parking area can be constructed to meet this requirement.
14.2.4.1 (viii)	Car Spaces for People with Disabilities	Complies 1 space is provided
14.2.4.1 (ix)	Reverse Manoeuvring	Complies Cars can manoeuvre on site in order to exit the site in a forward gear.
14.2.4.1 (xi)	Queuing	Complies Minimum queuing length of 6 metres is provided.
14.2.4.1 (xiv)	Surface of Parking and Loading Areas	Complies The parking area will be sealed. Car park surface will be sealed.
14.2.4.1 (xvi)	Landscaping	Complies The car parking and manoeuvring area is 364m ² , therefore 21.8m ² of landscaping is required. This is provided as part of this proposal



14.2.4.1 (xvii)	Illumination	Complies Exterior lighting will be designed to meet this standard.
14.2.4.2 (i)	Length of vehicle crossings	Complies Crossing length will be between 4-9 metres.
14.2.4.2 (ii)	Design of vehicle crossing	Complies The crossing is at 90 degrees to the road and property boundary.
14.2.4.2 (iii)	Maximum gradient for vehicle access	Complies The site is generally flat and can meet this standard.
14.2.4.2 (iv)	Minimum Sight Distances from Vehicle Access	Complies 80 metres sight distance is required and will be provided.
14.2.4.2 (v)	Maximum Number of Vehicle Crossings	Complies One vehicle crossing is proposed.
14.2.4.2 (vi)	Distances of Vehicle Crossings from Intersections	Complies 25m is required between the vehicle crossing and the nearest intersection. 42m is provided.

- 4.3 In summary, resource consent is required under the operative Plan as follows:
- Restricted Discretionary Activity, Rule 7.5.3.4 (i) Visitor accommodation activity in the low density residential zone. Discretion is in respect of:
 - (a) The location, external appearance and design of buildings;
 - (b) The location, nature and scale of activities on site;
 - (c) The location of parking and buses and access;
 - (d) Noise, and
 - (e) Hours of operation
 - Restricted Discretionary Activity, Rule 7.5.3.4 (vi), breach of site standards 7.5.5.2(iii) setback from roads and 7.5.5.2 (xiii) boundary planting. Discretion is confined to the matters specified in the standards not complied with.
 - Non-complying Activity, Rule 7.5.3.5, breach of zone standard 7.5.5.3(v) relating to building height and recession lines.
 - Restricted Discretionary activity, Rule 14.2.2.3 (ii), breach of site standards; 14.2.4.1(i) minimum parking space numbers, and 14.2.4.1 (v) size of parking spaces.
- 4.4 On the whole, the proposal should be considered as a **Non-Complying Activity**.

Queenstown Lakes Proposed District Plan

- 4.5 Although rules in the proposed District Plan do not yet have legal effect, it is considered appropriate to note key changes to the rules that relate to this site as these provide an indication of the direction of change in the District Plan and provide important context for the assessment of environmental effects. The operative Plan rules are around 20 years old and in this context, the “new” rules should better reflect the evolved environment over this considerable period of time.
- 4.6 Under the proposed District Plan the site is zoned Medium Density Residential. The key rule changes as relates to this site are discussed as follows (noting that a decision from the Council is expected in March 2018):
- 4.7 There is no rule controlling the height of boundary planting. This becomes a permitted activity.
- 4.8 The required setback from roads reduces from 4.5 metres to 3 metres. As the building is proposed to be setback 2 metres from both Dungarvon Street and Brownston Street, consent would still be required under this rule, however the degree of the rule breach would be reduced.

- 4.9 Recession planes undergo significant changes. Under the proposed plan, for this site, there are no applicable recession planes along road boundaries. At the southern boundary bordering 37 Dungarvon Street, the recession plane changes to 35 degrees (from 2.5 metres above ground level). At the western boundary bordering the YHA Backpackers at 90 Brownston Street, the recession plane changes to 45 degrees (from 2.5 metres above ground level). The proposed development would not breach these proposed recession planes.
- 4.10 The number of car parks required under the proposed plan is 15, and the development has would achieve this requirement. The proposed plan requires two mobility spaces, however one is provided.

National Environmental Standards

- 4.11 In terms of the proposal's compliance or otherwise with National Environmental Standards (**NES**), the only NES that is of potential relevance to this proposal is the NES for Assessing and Managing Contaminants in Soil to Protect Human Health.
- 4.12 In this instance, no evidence of the site being contaminated or potentially contaminated has been identified. As such, the NES for Assessing and Managing Contaminants in Soil to Protect Human Health is not considered applicable. Notwithstanding, any soil disturbance associated with the proposed development will be minor and associated with foundation construction, and as such would achieve compliance with the relevant NES soil disturbance requirements.

5. Assessment of Effects on the Environment

- 5.1 Section 88 of the Act requires that the Applicant undertake an assessment of any actual or potential effects on the environment that may arise from the proposal, and the ways in which any adverse effects may be avoided, remedied or mitigated.
- 5.2 This AEE addresses the potential environmental effects of the proposal in the following sequence:
- Visitor Accommodation Activity
 - Road Setback
 - Boundary Planting
 - Building Height and Recession Planes
 - Parking
 - Positive effects
- 5.3 Although the activity is to be considered in the round as a non-complying activity, it is considered that the matters of discretion identified in the operative District Plan provides a helpful framework for the consideration of effects.

Visitor Accommodation Activity

- 5.4 Rule 7.5.3.4 (i) identifies the following matters for consideration in respect of visitor accommodation activity:
- (a) The location, external appearance and design of buildings;*
 - (b) The location, nature and scale of activities on site;*
 - (c) The location of parking and buses and access;*
 - (d) Noise, and*
 - (e) Hours of operation*
- 5.5 The building has been architecturally designed, and extensive landscaping is proposed to help soften the appearance of the building from public view points, namely the adjoining roads. As identified above, the building is located within the 4.5m road setback (being 2m from the road boundary at its closest point); the effects of the building placement are discussed further below with reference to the specific assessment matter of relevance.

- 5.6 The site is located diagonally across the intersection from the Wanaka town centre. Other land use activities within a one block radius of the site include a supermarket, retail shopping facilities, three backpacker accommodation businesses, a cinema, a motel, restaurants, a guesthouse, a large park and a considerable amount of public vehicle parking. The proximity of the site to these and other facilities promotes walking rather than vehicle use, and supports the activity in this location. The nature of this motel development is compatible with the local area, which includes a variety of established accommodation businesses. The scale of the activity is necessary to ensure the viability of the motel business as well as being a practical number of units for the size of the site. The scale of the activity is proportionate to the scale of other similar activities in the locale.
- 5.7 Parking facilities have been located to the rear of the site to minimise effects on residential amenity and to ensure that the street scene is well presented rather than being dominated by vehicle parking and hard surfacing; this was an important factor in the design of the building and site layout. To accommodate a practical number of vehicle parks, the building positioning is required to breach the 4.5m road setback; however the outcome proposed is much more favourable and presentable than having the vehicle parking along the roadside, which was a fallback option for the Applicant. Access to the site will be via a new vehicle crossing that meets the design standards in the operative District Plan, ensuring that pedestrian and vehicle safety are appropriately provided for. Access and parking for buses has not been provided as bus groups are not anticipated to use this motel facility.
- 5.8 The motel facility will have a dedicated manager who will be responsible for ensuring that day to day operations meet the conditions of the resource consent. Noise will be managed on site by the motel management staff. There are no outdoor entertainment areas proposed which will minimise any noise audible in adjacent residential areas. The motel will operate 24 hours per day, however effects on residential amenity will be minimal due to the presence of on-site management, and no outdoor entertainment areas on site.

Road Setback

- 5.9 Under section 7.7.2 xvii(2), the relevant assessment matters are as follows:

- (a) The extent to which the intrusion into the street scene is necessary in order to allow more efficient, practical use of the remainder of the site.*
- (b) The extent to which the proposed building will detract from the coherence, openness and attractiveness of the site as viewed from the street and adjoining sites.*
- (c) The ability to provide adequate opportunity for landscaping in the vicinity of road boundaries, which will mitigate the effects of building intrusion into the street scene.*
- (d) The ability to provide adequate on-site parking and manoeuvring for vehicles.*
- (e) The extent to which the proposed building will be compatible with the appearance, layout and scale of other buildings and sites in the surrounding area, including the setback of existing buildings in the vicinity of road boundaries.*
- (f) The extent to which the proposed building will have size, form, proportions, roof line, style, external appearance which are similar to or in keeping with those existing buildings on the site.*
- (g) The extent to which the location of the proposed building would adversely affect the historic character of Arrowtown.*

- 5.10 The motel building is proposed to be setback from the road by two metres. The size of the building is necessary to enable to provision of sufficient rooms to enable this motel to be a viable business (assessment matter (a)). The positioning of the building on the site is essential to enable to provision of on-site parking with sufficient manoeuvring area (assessment matters (a) and (d)).
- 5.11 In terms of assessment matter (b), the building has been designed in order to ensure coherence and attractiveness from street and adjoining private viewpoints. The architect has intentionally aimed to accentuate the corner of the site as a feature of the design with the added landscaping to assist with this outcome. A professional design from a landscape architect has been sought to soften the appearance of the building with landscaping, mitigating the appearance of the building in the street scene (assessment matter (c)).
- 5.12 With regard to assessment matters (e), the locale comprises a mix of single storey and two-storey buildings. There are several two-storey buildings in the neighbourhood, including the neighbouring residence at 37 Dungarvon Street. Diagonally across the Dungarvon / Brownston intersection is the Wanaka Town Centre comprising several multi-storey commercial buildings in a dense arrangement, with minimal setback to the road boundary. Given the proximity to the town centre commercial buildings, and the variation of residential built form in the locale, the proposed development is considered to be compatible with the appearance, layout and scale of other buildings in the surrounding area. With the zoning change anticipated with the proposed District Plan, it is apparent that the neighbourhood will develop more two-storey buildings, and higher density of development.

- 5.13 There are no other buildings on the site which will be retained as part of the proposed development, and so assessment matter (f) is not considered to be relevant. Assessment matter (g) is not relevant as the site is not located in Arrowtown.
- 5.14 Overall, the effects of the building setback from the road are considered to be less than minor, and are sufficiently mitigated by the architectural design and proposed landscaping.

Boundary Planting

- 5.15 Rule 7.5.5.2 (xiii) requires that boundary planting shall not exceed 1.9 metres in height within 2 metres of the boundary. It is noted that this rule no longer exists in respect of this site in the proposed District Plan as notified, which provides a useful context in consideration of the evolution of the Plan. While the rule in the operative District Plan does require consideration, the lack of this rule in the proposed District Plan is a significant indication that the rule is not achieving the desired environmental outcomes.
- 5.16 There is an existing hedge along the south-eastern boundary that is proposed to be retained. Given the hedge is existing, there is no change in effect on neighbours or residential amenity as a result of this proposal. Retention of the hedge will act as a buffer between the neighbouring residential property and the motel, enhancing their residential amenity.
- 5.17 Trees that will eventually exceed 1.9 metres in height are proposed to be planted along the Dungarvon Street frontage. These trees have been proposed in order to soften the appearance of the building and enhance the amenity of the site. The trees will not create excessive shading and loss of vistas given their position abutting the north-eastern and north-western façades of the proposed building. The presence of the trees is considered to have an overall positive effect on the amenity of the site and streetscape.

Building Height

- 5.18 The operative and proposed District Plans set a maximum height limit of seven metres, and this motel is proposed to be 8.2m high at its highest point, with the bulk

of the roofline however around 6m to 7m in height. The height is best described as undulating.

5.19 Under section 7.7.2 xii, the relevant assessment matters are as follows:

- (a) Whether any earthworks have been carried out on the site prior to the date of notification (10 October 1995) that have lowered the level of the site.*
- (b) Whether there are rules requiring the site to be built up.*
- (c) With regard to proposals that breach one or more zone standard(s), whether and the extent to which the proposal will facilitate the provision of a range of Residential Activity that contributes to housing affordability in the District.*

5.20 The Applicant advises that no earthworks have been carried out on the site that have lowered the level of the site. There are no rules requiring the site to be built up. The proposal does not provide any residential housing.

5.21 The building is positioned close to both road boundaries, with parking and service area located to the rear of the property, in order to minimise effects of the building height on neighbours.

5.22 The purpose of the Medium Density Residential Zone in the proposed District Plan notes:

While providing for a higher density of development than is possible in the Low Density Residential Zone, the zone utilises development controls to ensure reasonable amenity protection is maintained. Importantly, building height will be generally limited to two storeys.

5.23 Two storey buildings are specifically anticipated in the proposed District Plan Medium Density Residential Zone, with the height control purposed to limit development to two storeys. The proposed motel complex is a two storey building, which exceeds the plan standard height limit as a result of the roofline forming an architectural feature. The roofline mimics the alpine mountain appearance and introduces visual interest and variety to the built form aesthetic, without adversely affecting privacy of the neighbouring properties. The proposed design adds greater amenity to the neighbourhood than a flat roof structure which complies with the building height limit.

Recession planes

5.24 The building will breach recession planes under the operative District Plan on all four sides, as illustrated in [Table 2](#) below, and shown on plans enclosed as **Attachment C**.

Table 2: Details of recession plane breaches

Boundary	Adjacent Land Use	Recession Plane Intersect with Building
North-east	Dungarvon Street	Approximately 3 metres above ground level for a building length of approximately 33.1 metres. The remainder of the building is set back far enough from the boundary such that it does not breach this recession plane.
North-west	Brownston Street	Approximately 3 metres above ground level for a building length of approximately 11.2 metres. The remainder of the building is set back far enough from the boundary such that it does not breach this recession plane.
South-east	Residence - 37 Dungarvon Street	5.5 metres above ground level for a building length of 8.5 metres. The remainder of the building is set back far enough from the boundary such that it does not breach this recession plane.
South-west	YHA Backpackers – 90 Brownston Street	4 metres above ground level for a building length of 8.6 metres. The remainder of the building is set back far enough from the boundary such that it does not breach this recession plane.

- 5.25 Under the proposed District Plan, the recession planes will be not be breached at all and this has been a relevant consideration in the building design; it would not be a sustainable land use to design the building to comply with the operative Plan recession plane standards when new rules are pending. Recession planes along the road boundaries are removed by the proposed District Plan, and the angle of recession planes along the other boundaries are steepened such that the proposed building fits within. The changes to the recession planes are a result of the proposed zoning change; the medium density residential zone specifically anticipates denser development, for example terrace housing, semi-detached housing and detached townhouses. The proposed motel building is comparable to these types of residential buildings.
- 5.26 The impact of the buildings on neighbour’s privacy will be less than minor. There are no windows on the second storey of the motel building on the sections of the building that are closest to each residential boundary. If the site was developed for residential purposes, there could conceivably be greater privacy impacts compared with the motel use.

- 5.27 Currently there is very little sunlight into the north-east facing windows at the YHA due to the presence of large shrubs between the boundary fence and the YHA building. The north western facing windows will be slightly more shaded during morning, but will continue to receive midday and afternoon sun. For the residence at 37 Dungarvon Street, effects of shading are minimised by the site layout with the driveway between the motel building and the property boundary, and parking located adjacent to that boundary. Some loss of north facing sunlight in the afternoon is possible, however morning sun into this property should be unaffected. The degree of effect on neighbour's sunlight is anticipated by the proposed District Plan, as the building design complies with the proposed recession planes.
- 5.28 An alternative design in order to comply with the operative District Plan recession planes would be a flat or monopitch roof, which is considered to be an inferior design outcome.
- 5.29 For the above reasons, the effect of the breach of recession planes is considered to be less than minor.

Parking

- 5.30 The operative District Plan requires two parking spaces per accommodation unit; a total requirement of 42 parking spaces. In order to efficiently use the land resource at the site, and given the site is within easy and accessible walking distance to local amenities, 15 parking spaces are proposed. Guests staying at the motel may not require a car as the town centre and other local amenities are within easy walking distance of the site.
- 5.31 On the rare occasion that the motel parking requirements may be greater than the number of spaces, there is a considerable amount of paid public parking in the area, as well as street parking. While it is not expected that street parking will be needed, any periodic use will be indiscernible.
- 5.32 The proposed District Plan sets a new expectation for car parking; 15 parking spaces would be required under the proposed plan, which is what is proposed in this application. The parking requirements in the operative District Plan are over twenty years old and it is considered that the standards in the proposed District Plan are more fitting for how the district has evolved since the operative District Plan rules

were written. The Applicant intends to make a submission on the proposed District Plan in support of the reduction in the number of parking spaces required.

- 5.33 Given the accommodation capacity proposed and the proximity of the site to the town centre, it is considered that the amount of parking proposed is sufficient for the site.

Positive effects

- 5.34 To ignore the positive effects of the proposal would be to overstate any adverse effects that may arise from the proposal. The proposed motel will add an accommodation option for visitors to Wanaka, and is ideally positioned for tourists adjacent to the town centre, Pembroke Park and other amenities. The proposal will allow development of a partially empty site, improving the cohesion and general amenity of the area.

Conclusion

- 5.35 On the basis of the above assessment, it is considered that the proposal will have less than minor adverse effects on the receiving environment, while having considerable positive effects. The motel is considered to be an appropriate development for the site and locale.

6. Statutory Assessment

Objectives and Policies

- 6.1 Section 104 of the Act requires that the relevant provisions of the relevant operative and/or proposed plan(s), or any other matter the consent authority considers relevant and reasonably necessary, to be considered when assessing an application. In this instance, the most relevant planning documents are the operative District Plan and the proposed District Plan.
- 6.2 The Otago Regional Policy Statement (“**CRPS**”) is also a relevant document. In the context of the proposal the more specific objectives and policies of the operative and proposed District Plans provide more informed guidance however we note for completeness and for the reasons outlined below, the proposal is consistent with those relevant objectives and policies in the ORPS.
- 6.3 The relevant operative District Plan objectives and policies are identified in Table 3, and the relevant Proposed District Plan objectives and policies are identified in Table 4, as follows.

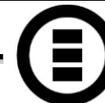
Table 3: Operative District Plan Objectives and Policies Assessment.

Objective	Policies	Assessment
Operative District Plan		
Chapter 7 – Residential Areas		
<p>7.1.2 (1) - Availability of Land Sufficient land to provide for a diverse range of residential opportunities for the District's present and future urban populations, subject to the constraints imposed by the natural and physical environment.</p>	<p>1.1 To zone sufficient land to satisfy both anticipated residential and visitor accommodation demand.</p> <p>1.2 To enable new residential and visitor accommodation areas in the District.</p> <p>1.3 To promote compact residential and visitor accommodation development.</p> <p>1.4 To enable residential and visitor accommodation growth in areas which have primary regard to the protection and enhancement of the landscape amenity.</p> <p>1.6 To promote, where reasonable, a separation of visitor accommodation development from areas better suited for the preservation, expansion or creation of residential neighbourhoods.</p>	<p>Consistent</p> <p>These policies demonstrate that the plan does anticipate some visitor accommodation activity in this zone. The proposal provides for a reasonably compact visitor accommodation development in an ideal location at the transition between a residential neighbourhood and the town centre.</p>
<p>7.1.2 (3) - Residential Amenity. Pleasant living environments within which adverse effects are minimised while still providing the opportunity for community needs.</p>	<p>3.1 To protect and enhance the cohesion of residential activity and the sense of community and well being obtained from residential neighbours.</p> <p>3.2 To provide for and generally maintain the dominant low density development within the existing Queenstown, Wanaka and Arrowtown residential zones, small townships and Rural Living areas.</p> <p>3.4 To ensure the external appearance of buildings reflects the significant landscape values and enhance a coherent urban character and form as it relates to the landscape.</p>	<p>Consistent</p> <p>The building design emphasizes the alpine landscape values. Hours of operation and noise will be actively managed on site in order to minimise any effects on residential amenity. Adequate on site parking is provided for the nature and location of the activity.</p>

Objective	Policies	Assessment
	<p>3.5 To ensure hours of operation of non-residential activity do not compromise residential amenity values, social well being, residential cohesion and privacy.</p> <p>3.6 To ensure a balance between building activity and open space on sites to provide for outdoor living and planting.</p> <p>3.7 To ensure residential developments are not unduly shaded by structures on surrounding properties.</p> <p>3.8 To ensure noise emissions associated with non-residential activities are within limits adequate to maintain amenity values.</p> <p>3.9 To encourage on-site parking in association with development and to allow shared off-site parking in close proximity to development in residential areas to ensure the amenity of neighbours and the functioning of streets is maintained.</p> <p>3.14 To distinguish areas with low density character where that character should be retained from areas of change located close to urban centres or adjacent to transport routes where higher density development should be encouraged.</p>	
<p>7.3.3 (1) - Residential and visitor accommodation development of a scale, density and character within sub zones that are separately identifiable by such characteristics as location, topology, geology, access, sunlight or views.</p>	<p>3 To provide limited opportunity for higher density residential development close to the Wanaka town centre.</p> <p>4 Residential development organised around neighbourhoods separate from areas of predominately visitor accommodation development.</p>	<p>Consistent</p> <p>The site is located immediately adjacent to the Wanaka town centre, and in proximity to multiple visitor accommodation activities.</p>



Objective	Policies	Assessment
7.3.3 (2) <i>Low density rural living development in identified locations in close proximity to Wanaka.</i>	5 <i>Avoid the planting and locating of inappropriate tree species so as to reduce the impact of excessive shading and loss of vistas.</i>	Consistent Trees proposed to be planted will be located so as not to create excessive shading or loss of vistas for neighbouring properties.
7.3.3 (3) <i>Retention of the general character of the residential environments in terms of density, building height, access to sunlight, privacy and views.</i>		
Chapter 14 – Transport		
14.1.3 (1) – <i>Efficiency</i> <i>Efficient use of the District’s existing and future transportation resource and of fossil fuel usage associated with transportation.</i>	1.3 <i>To promote the efficient use of roads by ensuring that the nature of activities alongside roads are compatible with road capacity and function.</i> 1.5 <i>To promote the efficient use of fuel for transport purposes, by providing for a District wide policy of consolidated urban areas, townships, retail centres and residential environments.</i> 1.6 <i>To promote and provide for the consolidation of new areas of residential development and for higher density development within identified areas.</i> 1.9 <i>To require off-road parking and loading for most activities to limit congestion and loss of safety and efficiency of adjacent roads and to promote the maintenance and efficiency of those roads.</i> 1.10 <i>To require access to property to be of a size, location and type to ensure safety and efficiency of road functioning.</i>	Consistent The location of the activity promotes walking and use of public transport, rather than vehicle use. Some on site parking is provided and vehicle access is designed in order to be compliant with the plan standards to ensure the safety and efficiency of the road network.
14.1.3 (2) - <i>Safety and Accessibility</i> <i>Maintenance and improvement of access, ease and safety of pedestrian and vehicle movement throughout the District.</i>	2.2 <i>To ensure the intensity and nature of activities along particular roads is compatible with road capacity and function, to ensure both vehicle and pedestrian safety.</i>	Consistent A mobility parking space has been provided for accessibility. Vehicle access has been designed to meet



Objective	Policies	Assessment
	<p>2.3 To ensure access and movement throughout the District, and more particularly the urban areas, for people with disabilities is not unreasonably restricted.</p> <p>2.6 To ensure intersections and accessways are designed and located so:</p> <ul style="list-style-type: none"> • good visibility is provided. • they can accommodate vehicle manoeuvres. • they prevent reverse manoeuvring onto arterial roads; and • are separated so as not to adversely affect the free flow of traffic on arterial roads. 	<p>plan standards in order to ensure pedestrian and vehicle safety. Vehicles are able to manoeuvre on site and will not need to exit the site in reverse gear.</p>
<p>14.1.3 (5) - Parking and Loading - General Sufficient accessible parking and loading facilities to cater for the anticipated demands of activities while controlling adverse effects</p>	<p>5.1 To set minimum parking requirements for each activity based on parking demand for each land use while not necessarily accommodating peak parking requirements.</p> <p>5.3 To ensure car parking is available, convenient and accessible to users including people with disabilities.</p> <p>5.4 To require all off-street parking areas to be designed and landscaped in a manner which will mitigate any adverse visual effect on neighbours, including outlook and privacy.</p> <p>5.5 To require the design of parking areas to ensure the safety of pedestrians as well as vehicles.</p>	<p>Consistent</p> <p>The number of parking spaces is considered to be appropriate for the nature of the activity on site, particularly given the proximity of the site to local amenities and public off-site parking facilities.</p>



Table 4: Proposed District Plan Objectives and Policies Assessment.

Objective	Policies	Assessment
Proposed District Plan		
Chapter 3 – Strategic Direction		
3.2.1.1 - Recognise, develop and sustain the Queenstown and Wanaka central business areas as the hubs of New Zealand’s premier alpine resorts and the District’s economy.	3.2.1.1.3 - Promote growth in the visitor industry and encourage investment in lifting the scope and quality of attractions, facilities and services within the Queenstown and Wanaka central business areas.	Consistent The establishment of this motel will allow visitors to stay in walking distance of the Wanaka town centre, promoting a positive visitor experience.
3.2.3.1 - Achieve a built environment that ensures our urban areas are desirable and safe places to live, work and play.	3.2.3.1.1 - Ensure development responds to the character of its site, the street, open space and surrounding area, whilst acknowledging the necessity of increased densities and some change in character in certain locations.	Consistent The development provides landscaping and in order to integrate into the surrounding environment, while being a more modern, denser development as anticipated by the proposed district plan.
Chapter 8 – Medium Density Residential		
8.2.2 - Development provides a positive contribution to the environment through quality urban design solutions which complement and enhance local character, heritage and identity.	8.2.2.1 - Buildings shall address streets and provide direct connection between front doors and the street, with limited presentation of unarticulated blank walls or facades to the street. 8.2.2.3 - Street frontages shall not be dominated by garaging, parking and accessways. 8.2.2.4 The mass of buildings shall be broken down through variation in facades and materials, roof form, building separation and recessions or other techniques to reduce dominance on streets, parks, and neighbouring properties. 8.2.2.5 - Landscaped areas shall be well designed and integrated into the design of developments, providing high amenity spaces	Consistent The design provides features of interest and landscaping along the road frontage.



Objective	Policies	Assessment
	<i>for recreation and enjoyment, and to soften the visual impact of development, with particular regard to the street frontage of developments.</i>	
<p>8.2.4 - Provide reasonable protection of amenity values, within the context of an increasingly intensified suburban zone where character is changing and higher density housing is sought.</p>	<p>8.2.4.1 - Apply recession plane, building height, yard setback, site coverage, and window sill height controls as the primary means of ensuring reasonable protection of neighbours' privacy and amenity values.</p> <p>8.2.4.2 Ensure buildings are designed and located to respond positively to site context through methods to maximise solar gain and limit energy costs.</p>	<p>Consistent The development complies with the proposed district plan recession planes. As discussed above the layout has been design so as to minimise any impact on neighbours privacy and amenity.</p>
<p>8.2.7 - Ensure medium density development efficiently utilises existing infrastructure and minimises impacts on infrastructure and roading networks.</p>	<p>8.2.7.1 - Medium density development is provided close to town centres and local shopping zones to reduce private vehicle movements and maximise walking, cycling and public transport patronage.</p> <p>8.2.7.2 - Medium density development is located in areas that are well serviced by public transport and infrastructure, trail/track networks, and is designed in a manner consistent with the capacity of infrastructure networks.</p> <p>8.2.7.3 Access and parking is located and designed to optimise efficiency and safety and minimise impacts to on-street parking.</p> <p>8.2.7.4 - A reduction in parking requirements may be considered in Queenstown and Wanaka where a site is located within 400 m of either a bus stop or the edge of a town centre zone.</p>	<p>Consistent The development is located close to the Wanaka town centre and will promote active transport and use of public transport.</p>
<p>8.2.14 - The location, scale and intensity of visitor accommodation, residential visitor accommodation and homestays is managed to maintain the residential character of the zone.</p>	<p>8.2.14.1 - Provide for accommodation options for visitors in the Medium Density Visitor Accommodation Sub-Zones and the Wanaka Town Centre Transition Overlay Sub-Zones that</p>	<p>Consistent The development provides for visitor accommodation in the medium density residential zone.</p>



Objective	Policies	Assessment
	<p><i>is appropriate for the medium density residential environment.</i></p> <p><i>8.2.14.2 - Restrict the establishment of visitor accommodation in locations outside the Medium Density Visitor Accommodation Sub-Zones and the Wanaka Town Centre Transition Overlay to ensure that the zone maintains a residential character and the supply of residential housing is achieved.</i></p>	



6.4 As shown through the above assessment, the proposed development at 86 Brownston Street is consistent with the relevant objectives and policies of the operative and proposed District Plans.

Section 95 of the RMA

6.5 Section 95A of the RMA sets out steps the consent authority must follow to determine whether to publicly notify an application for a resource consent. In regard to these steps, the following points are noted:

- The applicant has not requested that the application be publicly notified.
- There are no rules or national environmental standards that require or preclude notification.
- No special circumstances exist in relation to the application.
- In consideration of the assessment of effects, it is concluded that the proposal will result in less than minor adverse effects on the environment

6.6 Section 95E of the RMA requires a consent authority to decide if any persons are affected by a proposal. There are no parties considered to be affected by the proposal.

Purpose & Principles of the Resource Management Act

6.7 As determined in *RJ Davidson Family Trust v Marlborough District Council*, under Section 104 of the Act a decision-maker should not resort to Part 2 unless the relevant planning documents are invalid, have incomplete coverage, or have uncertain meaning. Regardless, Schedule 4 of the RMA requires an application for resource consent to include assessment against matters set out in Part 2.

6.8 The purpose of the RMA, as set out under section 5 (2) is to promote the sustainable management of natural and physical resources. The relevant matters in Sections 6, 7 and 8 of the RMA also require consideration. There are no matters of national importance under Section 6 that need to be recognised and provided for in this application.

- 6.9 The RMA specifies that regard must be had to the relevant matters listed in section 7. The relevant matters include:
- (b) The efficient use and development of natural and physical resources.*
 - (c) The maintenance and enhancement of amenity values.*
 - (f) Maintenance and enhancement of the quality of the environment.*

In the whole, the proposal is considered to efficiently use the natural (land) resource of the site, providing a development that will meet demand for visitor accommodation in Wanaka. The proposal will enhance amenity values and the quality of the environment through providing development and landscaping on an empty lot, adding vibrancy to the area.

- 6.10 There are no matters under Section 8 that require consideration with respect to this application.
- 6.11 For the reasons outlined in this report, the proposal is consistent with the purpose and principles under Section 5, and the associated matters under Part 2 of the Act. The proposal represents an efficient use of the land resource and will be undertaken in a manner which avoids, remedies and mitigates potential adverse effects on the environment.
- 6.12 The proposal is consistent with the purpose and principles of the Act and accords with the definition of sustainable management.

APPENDIX 3 – ENGINEERING REPORT



ENGINEERING REPORT

TO: Rosalind Devlin

FROM: Cameron Jones

DATE: 10/09/2019

APPLICATION DETAILS	
REFERENCE	RM180133
APPLICANT	C & J Limited
APPLICATION TYPE & DESCRIPTION	Land Use Consent is sought to construct a 21 unit motel.
ADDRESS	33 Dungarvon Street, Wanaka
ZONING	ODP: Low Density Residential PDP: Medium Density Residential
LEGAL DESCRIPTION	Sec 1 Blk XXIII Town of Wanaka
SITE AREA	1,002m ²
ACTIVITY STATUS	Restricted Discretionary

Application	Reference Documents	Documents provided with consent application.
	Previous Relevant Consents	None relevant.

Location Diagram



Comments	
Existing Use	Residential allotment with an existing residential unit.
Neighbours	Dungarvon Street to the east; Brownston Street to the north; backpackers to the west; occupied residential allotment to the south.
Topography/Aspect	Gently sloping down towards the northeast.

ENGINEERING		COMMENTS	Condition
TRANSPORT	Access	<p>The applicant proposes the construction of a total of 11 parking spaces on a flat sealed surface up to 1.45m below the existing ground level. I leave an assessment of the number of parking spaces to the planner.</p> <p>As stated above, the applicant proposes to seal the parking area, and I am therefore satisfied that compliance with Council's requirements for surfacing will be achieved. The purpose of the excavations in the south corner of the site is to ensure that the parking spaces are flat, and I am therefore satisfied that compliance with District Plan requirements in respect to gradients will be achieved.</p> <p>The parking spaces proposed are generally 5.0m long by 2.8m wide, with a 5.7m aisle width. This aisle width is less than that permitted by the Operative and Proposed District Plans, but the stalls have been widened slightly and the applicant has provided swept paths, prepared by Carriageway Consulting Ltd (CCL), demonstrating that vehicles can enter and exit the spaces using only one reverse manoeuvre.</p> <p>An unassigned area towards the north of the parking area has been provided, so that a vehicle can turn around in the event that there are no spare spaces in the parking area and leave onto Dungarvon Street forwards. I recommend that this area be clearly and permanently marked out as a no parking area.</p> <p>The Operative and Proposed District Plans require a minimum of 6m of queuing length, and this has been provided.</p> <p>I recommend a condition that the detailed design of the parking area be provided to Council for Engineering Acceptance prior to the commencement of works. I recommend a condition that the parking area be formed and sealed as per the accepted plans prior to use of the motel.</p>	X
	Means of Access	<p><u>Vehicle crossings / access</u></p> <p>The applicant is proposing the construction of a new vehicle crossing towards the south end of the Dungarvon Street frontage. I am satisfied that providing a vehicle crossing with compliant break over angles will be easily achievable, and the plans show that the access ramp will have a gradient of approximately 5° (or 1:11), in accordance with Council requirements. The vehicle crossing is approximately 5.5m long, complying with the requirements of the operative and proposed District Plans.</p>	

	Means of Access continued	<p>The sight distance available from the vehicle crossing to the northwest is approximately 66m, less than the 80m required by Site Standard 14.2.4.2.iv of the Operative District Plan and Rule 29.5.18 of the Proposed District Plan. The applicant has provided an assessment of this breach from CCL, which states that the sight distance is restricted by the intersection of Brownston Street / Dungarvon Street. As vehicles negotiating this intersection will have slowed down, CCL considers that the available sight distance is appropriate. I agree with this expert advice.</p> <p>As the existing vehicle crossing will no longer be required, I recommend a condition that it be removed and grassed berm reinstated. Further, the existing dropped kerb shall be removed and standard kerbing reinstated (including re-grading the footpath).</p> <p>I recommend a condition that the detailed design of the new vehicle crossing and removal of the existing vehicle crossing be provided to Council for Engineering Acceptance prior to works commencing. I recommend a condition that these works be completed prior to occupation of the motel.</p>	X
--	----------------------------------	---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	----------

ENGINEERING		COMMENTS	Condition	
EARTHWORKS	Extent	Description	Excavation is proposed to lower the finished floor level of the motel. This is to be retained along the north, east and south boundaries.	
		Cut /Fill Volume (m3)	From the architect's plans: 650m ³ total cut.	
		Total Volume (m3)		
		Area Exposed (m2)	It appears that the majority of the 1,002m ² site is affected.	
		Max Height Cut/Fill (m)	Max cut: 1.45m	
	Prox. to Boundary	<p>A maximum cut depth of up to 0.8m is proposed on the east and south boundaries, with permanent retaining walls proposed. The applicant has provided advice from Geotek Consulting indicating that this cut will not be stable, and temporary retaining is therefore required, Consequently, a temporary retaining design and construction methodology has been provided. I accept this advice, and recommend conditions requiring that the works be supervised by a suitably qualified person, and that the construction of the retaining walls be undertaken in accordance with this methodology.</p> <p>A maximum cut depth of up to 1.45m is proposed along the east (Dungarvon Street) boundary, with permanent retaining walls proposed. This will require temporary batter slopes into the Council's road reserve, which will require a temporary Licence to Occupy (LTO) as the duration of the works is fairly long (indicated to be 2-3 weeks). It is understood that acquiring this LTO would be a fairly straightforward process. The applicant has volunteered a condition that an appropriate LTO be procured prior to the commencement of works, and I recommend that this be provided as part of the Engineering Acceptance application.</p>	X	
	Stability	Geotech assessment by	None provided.	
		Rock breaking	Not anticipated.	
Rock blasting				

	Preconstruction survey	As the earthworks are in close proximity to neighbouring properties, I recommend an advice note with regard to undertaking a preconstruction survey.		
	Retaining	The plans indicate that the cut areas will be supported by low retaining walls. As the retaining walls are very close to the boundaries, I recommend appropriate conditions requiring the consent holder to submit PS1 and PS4 statements prior to commencing works and prior to occupation of the building, respectively. A condition is recommended to ensure that all retaining is constructed to Council's standards and an advice note is recommended in regards to Building Act requirements for retaining structures.	X	
	Recommendations on cut/batter slopes	Permanent batters to be no greater than 2(H):1(V).	X	
	Fill certification/specific foundation design required	Not required.		
	Engineers supervision	Required to ensure that the works on the south and east boundaries remain stable, and to require the use of temporary retaining.	X	
	Uncertified fill covenant	Not required.		
	Schedule Certificate 2a	Not required.		
	Clean fill only	Not required.		
	Site Management	Report reference	Appropriate conditions are recommended to ensure that the site management is undertaken in accordance with the QLDC's Land Development and Subdivision Code of Practice.	X
		Specific sedimentation management	Not required.	
		Specific stormwater management		
		Neighbours	I am satisfied that the earthworks are feasible and no adverse effects will result on neighbouring sites, subject to compliance with the conditions recommended herein.	
		Traffic management	Required.	X
		Construction crossing	An appropriate condition is recommended to ensure that a construction crossing is in place prior to works commencing.	X
Revegetation		An appropriate condition is recommended to ensure all exposed areas are stabilised or re-vegetated at the completion of earthworks.	X	

SERVICE	Existing Services	The site has existing connections to Council's water and wastewater reticulation, along with reticulated power and telecommunications connections. It is assumed that the existing residential unit disposes of stormwater to ground.	
----------------	--------------------------	---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--

	Water	Potable	The existing 20mm water connection to the site is likely to be inadequate, so a larger connection will need to be provided. I recommend that the existing connection be capped prior to use of the motel. I recommend a condition that the detailed design of the capping and new connection be provided to Council for Engineering Acceptance prior to the commencement of works. I recommend a condition that these works be undertaken prior to use of the building.	X
		Fire-fighting	There are adequate existing fire hydrants in the streets surrounding the site within 135/270m to ensure compliance with SNZ PAS 4509:2008. Based on the site's location, I am satisfied that an FW3 fire water classification will be available to the site in accordance with SNZ PAS 4509:2008. I make no recommendations in this regard.	
	Effluent Disposal	The existing residential unit is connected to Council's wastewater reticulation. However, given the building's age, the size of this connection is unclear. I recommend a condition that this be confirmed as part of the applicant's Engineering Acceptance application, and a larger connection provided if required.	X	
	Stormwater	The applicant is proposing to dispose of their stormwater to ground, and it is understood that there is good soakage available in the gravels in the area. As the Building Consent process only covers stormwater runoff from the buildings themselves, and the parking and access areas in this instance have an area more or less equal to the area of the building, I recommend a condition that the detailed design of the soakage proposed be provided to Council for Engineering Acceptance prior to the commencement of works. I recommend a condition that the soakage be installed as per the accepted design prior to occupation of the building.	X	
	Power & Telecoms	The lot currently has connections to power and telecommunications reticulation. I am satisfied that appropriate connections to these services will be a requirement of the Building Consent process and I make no recommendations in this regard.		

PROJECT INFORMATION	Developers Engineering Representative	Required.	X
	Notice of commencement	Not required.	
	Traffic Management Plan	Required for works affecting the road reserve.	X
	Design Certificates	Required.	X
	Completion Certificates	Required.	X
	As built	Required for upgraded water connection.	X

TITLE	Covenants/consent notices	There are no existing consent notices or s108 covenants registered on the lot's title.	
-------	---------------------------	----------------------------------------------------------------------------------------	--

RECOMMENDED CONDITIONS

It is recommended that the following conditions are included in the consent decision:

General

1. All engineering works, including the construction of retaining walls, shall be carried out in accordance with the Queenstown Lakes District Council's policies and standards, being QLDC's Land Development and Subdivision Code of Practice adopted on 3rd May 2018 and subsequent amendments to that document up to the date of issue of any resource consent.

Note: The current standards are available on Council's website via the following link: <http://www.qldc.govt.nz>

To be completed prior to the commencement of any works on-site

2. The consent holder shall obtain and implement a traffic management plan approved by Council prior to undertaking any works within or adjacent to Council's road reserve that affects the normal operating conditions of the road reserve through disruption, inconvenience or delay. The Traffic Management Plan shall be prepared by a Site Traffic Management Supervisor (STMS). All contractors obligated to implement temporary traffic management plans shall employ a qualified STMS to manage the site in accordance with the requirements of the NZTA's '*Traffic Control Devices Manual Part 8: Code of practice for temporary traffic management*'. The STMS shall implement the Traffic Management Plan. A copy of the approved plan shall be submitted to the Manager of Resource Management Engineering at Council prior to works commencing.
3. The owner of the land being developed shall provide a letter to the Manager of Resource Management Engineering at Council advising who their representative is for the design and execution of the engineering works and construction works required in association with this development and shall confirm that these representatives will be responsible for all aspects of the works covered under Sections 1.7 & 1.8 of QLDC's Land Development and Subdivision Code of Practice, in relation to this development.
4. Prior to commencing works on the site, the consent holder shall obtain 'Engineering Review and Acceptance' from the Queenstown Lakes District Council for development works to be undertaken and information requirements specified below. The application shall include all development items listed below unless a 'partial' review approach has been approved in writing by the Manager of Resource Management Engineering at Council. The 'Engineering Review and Acceptance' application(s) shall be submitted to the Manager of Resource Management Engineering at Council for review, prior to acceptance being issued. At Council's discretion, specific designs may be subject to a Peer Review, organised by the Council at the applicant's cost. The 'Engineering Review and Acceptance' application(s) shall include copies of all specifications, calculations, design plans and Schedule 1A design certificates as is considered by Council to be both necessary and adequate, in accordance with Condition (1), to detail the following requirements:
 - a) The provision of a water supply to the development. This shall include an approved valve and valve box with backflow prevention and provision for water metering to be located at the road reserve boundary. The backflow prevention device shall be located on the subject site. The existing water connection shall be removed and capped at the main. The costs of the connection shall be borne by the consent holder.
 - b) The provision of a foul sewer connection to the development. In the event the existing wastewater connection becomes redundant it shall be removed and capped at the main. The costs of the connection shall be borne by the consent holder.
 - c) The provision of a stormwater collection and disposal system which shall provide both primary and secondary protection for future development within the development, in accordance with Council's standards and connection policy. This shall include:
 - (i) The provision of soak pits/infiltration galleries adequate to dispose of the runoff from the development during the critical 5% AEP storm event. Percolation testing shall be undertaken by a suitably qualified professional at the individual soak pit/infiltration gallery locations to adequately demonstrate that soakage is available in all areas proposed for soakage. The method for soakage testing is to be agreed with the Manager of Resource Management Engineering at Council prior to testing. A copy of the test results shall be provided to the Manager of Resource Management

Engineering at Council along with the design of the soak pits/infiltration galleries based on the percolation testing results. The soak pit/infiltration gallery design shall be in general accordance with the "Acceptable Solutions and Verification Methods for New Zealand Building Code Clause: E1/VM1 Surface Water," except for using the 5% AEP storm event, taking into account the critical storm duration and ensuring the soak pits/infiltration galleries will drain within 24 hours of the end of the critical event.

- (ii) A secondary protection system consisting of secondary flow paths to cater for the 1% AEP storm event and/or setting of appropriate building floor levels to ensure that there is no inundation of any building, and no increase in the effects due to run-off onto land beyond the site from the pre-development situation.
 - d) The provision of PS1 Producer Statement for design from a suitably qualified Chartered Professional Engineer for the retaining wall near the southeast and southwest boundaries. This shall include plans demonstrating that the retaining wall and any required drainage are located wholly within the site.
 - e) The provision of PS1 Producer Statement for design from a suitably qualified Chartered Professional Engineer for the retaining wall near the Dungarvon Street road reserve boundary, confirming that the wall has been designed to withstand a suitable surcharge load from vehicular traffic. This shall include plans demonstrating that the retaining wall and associated drainage are located wholly within the site.
 - f) The provision of an approved temporary 'Licence to Occupy' from the Council for all temporary works and occupations (i.e. temporary batter slopes and hoarding) required in Council's road reserve during the motel's construction.
 - g) The provision of a sealed vehicle crossing that shall be constructed to the development to Council's standards.
 - h) The removal of the existing vehicle crossing to the site. This shall include:
 - (i) Reinstatement of grassed berm between the footpath and the property boundary.
 - (ii) Removal of the existing dropped kerb and provision of kerbing commensurate with the surrounds.
 - (iii) Regrading of the footpath at the location of the existing vehicle crossing.
 - i) The construction and sealing of all vehicle manoeuvring and car parking areas to Council's standards. This shall include:
 - i. Parking and loading spaces shall be clearly and permanently marked out.
 - ii. The area required for turning at the north end of the parking area shall be clearly and permanently marked as a no parking area.
 - iii. All signage and markings are to be in compliance with the NZTA Manual of Traffic Signs and Markings.
 - iv. Provision shall be made for stormwater disposal.
 - j) The provision of Design Certificates for all engineering works associated with this development submitted by a suitably qualified design professional (for clarification this shall include all Roads, Water, Wastewater and Stormwater reticulation). The certificates shall be in the format of the QLDC's Land Development and Subdivision Code of Practice Schedule 1A Certificate.
5. Prior to commencing any work on the site the consent holder shall install measures to control and/or mitigate any dust, silt run-off and sedimentation that may occur, in accordance with QLDC's Land Development and Subdivision Code of Practice to ensure that neighbouring sites remain unaffected from earthworks. These measures shall be implemented **prior** to the

commencement of any earthworks on site and shall remain in place for the duration of the project, until all exposed areas of earth are permanently stabilised.

6. At least 7 days prior to commencing excavations, the consent holder shall provide the Manager of Resource Management Engineering at Council with the name of a suitably qualified geotechnical professional as defined in Section 1.7 of QLDC's Land Development and Subdivision Code of Practice who shall supervise the excavation procedures and retaining wall construction. This engineer shall continually assess the condition of the excavation and shall be responsible for ensuring that temporary retaining is installed wherever necessary to avoid any potential erosion or instability.

To be monitored throughout earthworks

7. No permanent batter slope within the site shall be formed at a gradient that exceeds 1(V):2(H).
8. The excavation and retaining wall construction near the southeast and southwest boundaries shall be undertaken in accordance with the recommendations of Geotek Consulting (email dated 8 September 2019 from Steven Roberts to Cam Jones 'Re: Fw: Wanaka Motel Geotech (2002-17).')
9. Temporary retention systems shall be installed wherever necessary immediately following excavation to avoid any possible erosion or instability.
10. The consent holder shall implement suitable measures to prevent deposition of any debris on surrounding roads by vehicles moving to and from the site. In the event that any material is deposited on any roads, the consent holder shall take immediate action, at his/her expense, to clean the roads. The loading and stockpiling of earth and other materials shall be confined to the subject site.
11. No earthworks, temporary or permanent, are to breach the boundaries of the site, except those required for the removal of the existing vehicle crossing, provision of a new vehicle crossing and services, and as permitted by an approved Licence to Occupy.

To be completed before occupation of the building

12. Prior to the occupation of the building, the consent holder shall complete the following:
 - a) The submission of 'as-built' plans and information required to detail all engineering works completed in relation to or in association with this development at the consent holder's cost. This information shall be formatted in accordance with Council's 'as-built' standards and shall include all Water, Wastewater and Stormwater reticulation (including private laterals and toby positions).
 - b) The completion and implementation of all reviewed and accepted works detailed in Condition (4) above.
 - c) An Elster Helix 4000 or C4000 / 4200 or Sensus Meitwin; Meistream; WP water meter shall be installed on to the Acuflo manifold as per Condition (4a) above.
 - d) The provision of PS4 Producer Statement for design from a suitably qualified Chartered Professional Engineer for the retaining wall near the southeast and southwest boundaries. This shall include plans demonstrating that the retaining wall and any required drainage are located wholly within the site.
 - e) All earthworked areas shall be top-soiled and revegetated or otherwise permanently stabilised.
 - f) The consent holder shall remedy any damage to all existing road surfaces and berms that result from work carried out for this consent.
 - g) The submission of Completion Certificates from both the Contractor and Accepted Engineer for all infrastructure engineering works completed in relation to or in association with this development (for clarification this shall include all Roads, Water, Wastewater and

Stormwater reticulation). The certificates shall be in the format of the QLDC's Land Development and Subdivision Code of Practice Schedule 1B and 1C Certificate.

Ongoing Conditions/Covenants

13. In the event that the Engineering Acceptance issued under Condition (4) contains ongoing conditions or requirements associated with the installation, ownership, monitoring and/or maintenance of any infrastructure subject to Engineering Acceptance, then at Council's discretion, a Covenant in Gross (or other alternative legal instrument acceptable to Council) shall be registered on the relevant Records of Title detailing these requirements for the lot owner(s). The final form and wording of the document shall be checked and approved by Council's solicitors at the consent holder's expense prior to registration to ensure that all of the Council's interests and liabilities are adequately protected. The applicant shall liaise with the Subdivision Planner and/or Manager of Resource Management Engineering at Council in respect of the above. All costs, including costs that relate to the checking of the legal instrument by Council's solicitors and registration of the document, shall be borne by the applicant.

[Note: This condition is intended to provide for the imposition of a legal instrument for the performance of any ongoing requirements associated with the ownership, monitoring and maintenance of any infrastructure within this development that have arisen through the detailed engineering design and acceptance process, to avoid the need for a consent variation pursuant to s.127 of the Resource Management Act].

14. On completion of earthworks, a covenant pursuant to Section 108(2)(d) of the Resource Management Act 1991 shall be registered on the Register of Title of the subject site providing for the performance of any ongoing requirements for protection of secondary flow paths or minimum floor levels for buildings, where deemed necessary by Council to satisfy Condition 4(c)(ii) above. The final wording of the instrument shall be checked and approved by the Council's solicitors at the consent holder's expense prior to registration to ensure that all of the Council's interests and liabilities are adequately protected.

Advice Note:

1. This consent triggers a requirement for Development Contributions, please see the attached information sheet for more details on when a development contribution is triggered and when it is payable. For further information, please contact the DCN Officer at QLDC.

Prepared by:



Cameron Jones
LAND DEVELOPMENT ENGINEER

Reviewed by:



Lyn Overton
SENIOR LAND DEVELOPMENT ENGINEER

APPENDIX 4 – URBAN DESIGN REPORT



URBAN DESIGN SPECIALIST MEMORANDUM

To: Simon Childs, Planner
Planning & Development
Queenstown Lakes District Council

From: Erin Quin, Urban Designer
Vivian+Espie Limited.
Consultant to Planning & Development,
Queenstown Lakes District Council

Date: 3 December 2018

Subject: Request for Expert Advice - Urban Design
Proposed Motel at 86 Brownston Street, Wanaka

Application #: RM180133

Dear Simon,

Thank you for the opportunity to review the proposal for the proposed motel at 86 Brownston Street, Wanaka in the Low Density Residential zone of the Operative District Plan and the Medium Density Residential Zone of the Proposed District Plan.

The following information as part of the application has been reviewed in relation to my assessment:

- Urban Design Panel Minutes brought to the urban design panel on the 5th of April 2018, the minutes of which are appended as Attachment A;
- Assessment of Environmental Effects and Response to Further Information request collated and prepared by Town Planning Group;
- Architectural drawings, Design Statement and revised drawings prepared by Walker Architecture;
- Landscape plans prepared by Rough and Milne Landscape Architects;
- Parking Assessment prepared by Carriageway Consulting.

I have reviewed the application with regard to urban design best practice and the relevant QLDC District Plan objectives, policies and rules, and associated assessment criteria. Also of relevance to my assessment of applications is the MfE Urban Design Protocol, the Queenstown Lakes Urban Design Strategy and QLDC

Southern Lights Strategy. I have also reviewed the application in response to best practice Crime Prevention Through Environmental Design (CPTED) outcomes and the exterior treatment of the sites design development in response to universal access.

Response to Panel Concerns

At the time in which the panel reviewed the initial application, the issues were grouped into 3 categories, being those which are fundamental, those which are significant and other issues which need addressing (refer Attachment A).

The proposed design is considered below in relation to the panel's initial comments;

Fundamental

1. *Articulation. The panel suggested that the extent of the screening could be reduced and more variety added to the layout in order to reduce the apparent bulk.*

The screening which extends from the roof across the first floor elevations has been amended slightly with reduced density, however the first floor of the building as proposed retains a top heavy appearance accentuated by this proposed screening and is visually dominant particularly in combination with the reduced setback and breach in recession lines. The elevations submitted do not show the screening in great detail, however in combination with the screening on the ground floor level appear like 'jail cell' bars. The accentuated roofline and wing walls which demarcate vertically in rhythm along the street elevations are also visually lost in relation to this screening. It is recommended that the exterior screening on the first floor be removed in entirety or be re-designed to reduce the bulk form that remains dominant and contributes to the bulk form of the proposal.

Due to the accommodation only being short-term and as they are first floor units, I would consider that permanent fixtures on the exterior of the building for privacy unnecessary (i.e. unmoveable screening), and that this structure also inhibits the view out of the units. Privacy of the first floor units could instead be managed through interior design measures.

Overall, I do not consider this concern raised by the panel to be sufficiently resolved for urban design support and recommend removal of the screening along the first floor elevations.

2. *Maximum height – roof form design. The panel suggested the pitch and roof build up could be looked at to reduce the overall height benefiting both the buildings appearance and the character of the streetscape.*

The roof form design has been amended as recommended by the panel which has resulted in further reduction of the overall height of the building. The variation in roof form attempts to reduce the bulk form of the building and provides improved visual interest and architectural character. Although the building will

infringe maximum height, the form of building will be of two storey visitor accommodation, a typology not out of character in this zone or of the Medium Density Residential zone proposed.

3. *Screening – density reducing interaction with street and making first floor appear heavy. The panel suggested opening up the spacing of the screens to provide hints of occupancy whilst still providing privacy.*

As per comment one above. This matter is unresolved.

4. *Façade treatment – deliberate 2 dimensional approach resulted in accentuating bulk form of the building. The panel suggested the first floor screens didn't need to be in the same line as the building, i.e. could step in and out and at angles to the building for variation.*

The screening has previously been discussed above. The elevations do not clarify whether the first floor screens alternate at angles to provide additional variation and appear to continue to be in the same line as the building and continue to contribute to accentuating the bulk form. The gap and size variation of the slats do not in my opinion greatly contribute to the form of the building and its presence in the streetscape. Aside from the screening proposed, the elevations are well composed with windows overlooking the street environs; architectural features forming both rhythm and symmetry as well as variation. The controlled use of materials provides character without visually cluttering the façade.

5. *Privacy – The panel suggested privacy be looked at as rooms are 2 metres from busy footpath and street.*

I do not consider the proposed screening and landscaping in relation to the street to be fully resolved in terms of design to produce both amenity and privacy.

The landscape plan identifies that retaining and vertical timber fin fencing is proposed between 1.2-1.8m high. No further details or imagery have been submitted which provide detailed design of this fencing which has the potential of being visually dominant against the streetscape.

Landscape planting is proposed between the units and the fencing which will provide privacy and amenity to the units. However, the amenity contributed to the streetscape remains minimal consisting of a small area of *Pseudopanax Crassifolius* (Lancewood/Horoeka) with underplanting. In time the canopy of a row of *Sophora Microphylla* (Kowhai) which if successful grow to their full height of 8m, will contribute to street amenity and buffer the bulk form of the proposed buildings.

Although demarcation between the front of the proposed motel units and the street is supported and privacy of the ground floor units will be sufficient through the installation of the fencing proposed, the detrimental effect of the extent of the screen fencing on the boundary along the streetscape is considered significant and requires further consideration and design resolve. This specifically relates to the placement of the fencing in relation to the landscaping proposed and the detailed design of the fencing itself. The proposed

fin fencing could be an option provided confirmation that there are sufficient gaps between the timber fins is confirmed (informal surveillance) and the fencing is further differentiated at regular intervals to visually reduce the extent of this design feature. The proposed height of the fencing should be reduced in height to an overall no higher than 1.5m (inclusive of retaining) from street footpath level to improve the sightlines and activation of the street.

It is recommended that the front landscaping treatment be further resolved in terms of screen fencing and additional low level planting between the screening and the street to improve the street edge condition in regard to amenity and softening of the fence screening treatment.

Significant

6. *Passive surveillance of rear area – The panel suggested some openings to provide passive overlooking of the carpark and circulation areas.*

The units have been designed for outlook primarily onto the street environment. There are significant CPTED issues with the current design of the rear carpark area, which has low levels of informal surveillance, 24/7 open access (to the best of my knowledge) and no security access proposed.

Overlooking of this space could be improved by re-designing the office reception layout in relation to the storage/laundry space which currently blocks visual and physical access to the carpark.

The lack of informal surveillance opportunities into the carpark space from the buildings has the potential to increase the opportunities for crime. No significant changes of the proposal were amended in relation to greatly improving the passive surveillance of this area, however it has been confirmed that vision panels will be installed in all motel unit doors. This was not highly evident on the plans submitted. It is highly recommended that this be ensured through a consent condition for safety measures, and that at the very least the reception door leading from the carpark space be fully glazed. This will also aid in differentiating the entrance to the reception from the other motel unit doors, the entrance of which could architecturally be further pronounced for ease of wayfinding and legibility. The *Grisellinia Littoralis* hedging beside the disability carpark which I anticipate has been included mainly for screening should also be removed to allow for a small strip of informal surveillance from the street into the carpark area and also to improve access measures from the disability carpark. The removal of this small area of planting will not significantly reduce amenity.

No lighting details were submitted so it remains unclear whether this area will be sufficiently well lit to provide safety.

Overall, it should be noted that the rear area of the proposal will potentially be the most utilised space in terms of activity and passive surveillance of this space is considered to be poor.

7. *Different treatment of office street façade to motel units was suggested – larger area of glazing for active frontage and legibility of main entrance for both elevations as well as potential to open up to the rear area providing passive surveillance and security. Panel noted that in its location it had the best views and would be best shifted potentially to unit 04 so as to be centrally located between the two blocks.*

The office to the motel premises has been shifted to the ground level corner space providing street activation onto both Brownston and Dunmore Streets. The building layout however remains unresolved in relation to ease of wayfinding to the reception office from the rear of the site which is hindered through the placement of the storage and laundry area. This could be improved by enhancing prominence of the pedestrian entrance to the office/reception area from the carpark.

Other Issues

8. *Planting and landscaping – careful design/selection required to allow visual screening and to prohibit pedestrians walking up to the motel unit windows off public footpath*
9. *Landscaping to carpark - careful design/selection with regard to limited depth available for parking and vehicle movement.*
10. *Carpark design consideration – turning area for movement back out of site if carpark is full, space for idle waiting/loading near office, disabled carpark aisle*
11. *Lighting – CPTED and night time design of lighting to add life to the building at night.*

No lighting plans have been provided for review, however it is anticipated that this could be managed by means of condition with respect to sufficient lighting for safety and amenity. There is the opportunity for subtle yet effective lighting on the exterior of the building which could 'add life' to the building at night. It is recommended that a lighting plan be submitted for urban design approval in direct relation to safety and amenity.

As discussed previously, the submitted information and plans do not clearly resolve the concerns raised with respect to detailed design of the proposed screening and landscaping in relation to the streetscape the extent of which I am concerned will contribute to visual dominance and potentially a poor urban design outcome.

The planting and landscaping options have been considered in the proposed design which will allow for partial screening of the first floor units for privacy and a diverse native planting palette contributing to both amenity and ecological diversity. The screening proposed in relation to the planting in front of the units are a step toward achieving sufficient screening for privacy as well as landscape amenity to soften the built form, however further design resolve would improve the proposal's relationship to the street.

The proposed fencing along the Dungarvon and Brownston Street boundaries should be reconsidered to reduce the bulk form of the proposal in relation to the street whilst still providing privacy screening. The current design response has a landscaped yard along the street frontage for outlook purposes only and not for guests to use. Whilst I consider that outdoor areas in studio units intended for motel use may not be necessary in this location due to the close proximity of the site to parks and reserves of high amenity that the visitors can use and enjoy, the planting buffers provide minimal wider amenity which is under utilising the space and providing minimal contributions to street amenity. This problem can be easily resolved by shifting the screening closer to the units within the proposed landscaping area, so that both the guests and the public can appreciate the planting measures.

It is also recommended that the *Sophora Microphylla* (Kowhai) be planted on the street side of the proposed fencing to reduce the potential visual monotony of the screening and further detract from the effects of site standard infringements.

Assessment – Visitor Accommodation in the Low Density Residential Zone

I concur with the AEE that visitor accommodation in terms of landuse context and location is a suitable and efficient use of the site due to the close proximity to the Wanaka Town Centre and other amenities.

a) The Location, external appearance and design of buildings;

The architectural composition of the street front elevations along Dungarvon and Brownston Streets have been developed since the urban design panel session. The built form within the setback at two levels contributes to a strong built edge condition supported by glazing which overlooks the street environment. Importantly, the elevations have been altered to reduce the height of the building, reduce the density of the screening on the first floor and further vary the pitch of the roof.

The overall site layout concentrates the built form along the Dungarvon and Brownston Streets with a landscape buffer along the site boundaries to create both site and street amenity, to visually soften the built form as proposed, and to provide some separation and privacy between the street and the ground floor units. I note that although indicatively illustrated on the plans, no signage is formally proposed for review in the AEE which will have an effect on the overall reception of the development.

The overall landscape planting treatment is of native species which are attractive, low maintenance and contribute to native diversity. The low schist walls also contribute to retaining the vernacular of the District. The paving treatment to the public footpath should be consistent in materiality to reduce the potential for trip hazards and improvement of universal access measures. Whilst the bluestone pavers will sit flush against the exposed concrete aggregated paths and aid in providing a visual and textural cue to the motel site, I

recommend removal of the separated paver treatment across the grassed berm area and that this be replaced with exposed aggregate.

- *Breach in Height and Recession Lines*

It is noted that no changes are proposed in the PDP to the maximum building height of 7m. Although the roofline has purposefully been designed to rise and fall in a staggered manner, the proposed activity will still breach this by 1.2m at its highest point. The effect of the height breach is for only a small section of the roof on proposed unit 13 and will not result in any significant shading or dominance. I consider this proposed breach in height as no more than minor.

The majority of the recession lines are due to the placement of the building in relation to the street boundaries. This can be supported provided further design changes are made to improve façade treatment (by removing the fin screening on the first floor), and the amendment of the ground level fence screening and landscape planting in relation to street amenity.

b) The location, nature and scale of activities on site;

The proposed activity in terms of landuse in relation to urban design context is supported. The corner site in close proximity to the town centre is an ideal location for visitor accommodation and the site layout of the building in relation to the street with the parking to the rear, is supported.

Whilst the design of the parking area to the rear could be improved in relation to pedestrian access between the carparks themselves and the building, the carpark is spatially quite tight as proposed so an allowance for this not currently possible. The use of gobi pavers or a similar textured surface is highly supported as a traffic slowing measure across the extent of the carpark provided this is not used for the disability park where ease of access is paramount.

c) The location of parking, buses and access;

The location of the access into the rear carpark area with separate pedestrian access to the building and reception off the street is supported. I would consider that there may be some effects in terms of amenity and noise particularly on adjoining neighbours due to the number and frequency of travel movements anticipated with a motel operation.

As stated, the location of parking to the rear of the building is supported, however it is noted that the parking shortfall is significant and no travel plan information was submitted as part of the application (to the best of my knowledge), which suggests that guests will arrive by alternative modes of transport. There is no room for bus parking to be accommodated on the site and no cycle parking proposed. The site is relatively level from the street however proposed site works will drop the ground level to accommodate the activity. The

access to the site from the street is suitable, with 3 pedestrian links and the driveway kept separate to avoid a conflict in passage and safety. As noted previously, best urban design practice would also address the carpark area to separate the passage of vehicles and pedestrians for safety and amenity, however it is noted that an external paved area extends along the front of the motel units for pedestrian access to the units and although not ideal could also be used as pedestrian refuge when crossing and navigating the carpark.

d) *Noise, and*

e) *Hours of Operation*

The motel service itself is a 24 hour/7 days per week operation. Further information was requested of the applicant in regard to whether or not access would be restricted to the carpark area to which the response was that no restrictions in access were proposed. I note that a noise assessment was requested to be prepared as part of the applications review but I have not been privy to the findings of this assessment. Visitor accommodation is an anticipated outcome of the Proposed Medium Density zone, however given the scale of operation proposed I would anticipate the carpark area to be frequently used by guests. The design of the proposal also means that the main entrances to the units are internal to the site and therefore noise by guests is an anticipated outcome from both ground and the first floor. Proposed horizontal timber slat fencing to boundary is proposed, however, the acoustic properties of this feature has not been confirmed. I would anticipate the proposal having some effects on adjoining properties to the southeast and southwest.

- *Breach in Site Standards – Minimum Parking Space Numbers and Size of Parking Spaces*

Overall, it is noted that the application has Non-Complying Activity status due to the shortfall in carparking proposed. The Operative District Plan requires 2 carparks per unit, however it is noted that these rules were collated some time ago and that the context has changed. The proposal relies heavily on the Proposed District Plan provisions that allow for a significantly reduced number of carparks with the recognition that areas of higher density in close proximity to transport options, facilities and amenities will enable reduced demand of private vehicle trips thereby encouraging walking and the use of private transport.

Although the correspondence suggests that initial concerns raised with regard to parking demand is anecdotal only, and also post review of the transport report prepared by Mr Andy Carr of Carriageway Consulting, I would support a peer review of the findings in the transport report submitted as part of the application which I consider to be incomplete. No details were provided in the report which clarified a travel plan (i.e. the anticipated number of guests, peaks and troughs in accommodation anticipated, number of guests anticipated to arrive by private vehicle or otherwise), however, it was identified that one of the parks would be used for a manager which further reduced guest parking numbers, and no mention of parking for servicing or staff was reported, which further contributes to on-street parking.

A motel by common definition is visitor accommodation primarily for motorists. The AEE comments that for reasons of economic viability the proposed activity must accommodate 21 studio units although only 15 carparks are provided, one being a disability carpark (thereby restricted use only) and another used for motel management reducing the number of carparks readily available to 13. It still remains unclear as to the certainty in parking demand generated by the activity and I am unable to determine whether or not this would create significant adverse effects based on the information provided. However, I consider that the proposal will increase the demand for on-street parking in an area on the fringe of the Wanaka town centre that at times (to my understanding) is already in reasonably high demand, particularly in the peak Summer season.

As noted in the AEE, the site is opposite the town centre zone and is within walking distance of the town centre and so I would consider the streets in the vicinity to already be in high demand for parking, which is increased further during peak visitor seasons particularly due to its location (i.e. high visitor demand in the area, opposite Pembroke Park – a reserve frequently used for organised sports and events, and close to the lakefront).

I recommend that a council appointed transport review of the proposal be undertaken to further determine if the spillover parking would be significant as this potentially may result in significant adverse effects on the street and residential amenity.

Overall, it remains unclear as to whether the proposed activity will generate parking numbers that would have significant detrimental effect to on-street parking demand, however, based on the Operative District Plan provisions I would consider the shortfall significant. I do recognise that the parking provisions are in the process of changing, however the cumulative effect of approving a parking shortfall to this extent may be significant. It is noted that a reduction in the number of units that would reduce the extent of parking infringement (although not considered economically viable by the applicant) may be a supportable alternative.

Proposed Zoning

Through the District Plan review process the site is proposed to be rezoned as Medium Density Residential which to the best of my knowledge is under appeal. Relevant to urban design considerations of the proposal is the purpose of the proposed zoning which reads as follows;

Development will be required to achieve high standards of urban design, providing site responsive built forms and utilising opportunities to create vibrant public spaces and active transport connections (walking and cycling).

Overall, the development is working towards the intention of the purpose outlined above with respect to a high standard of urban design, however some minor design changes would significantly improve the proposal in terms of urban design benefit. The significance of the shortfall in carparking in terms of effects are matters that are still unconfirmed.

Summary of Recommendations

Parking Shortfall

Prior to approval, I recommend that a council appointed transport review of the proposal be undertaken to further determine the true effects of the spillover parking as this potentially would result in significant adverse effects on the street and residential amenity.

Building

- The screening fins which extend from the roof across the first floor windows are unnecessary and should be removed in entirety. These negatively accentuate the overall bulk form of the building and reduce the active engagement of the first floor units with the wider environment inclusive of the street. The fins also disturb the views of guests to the wider environment. Privacy matters, if required, can be resolved through interior design measures which are considered more appropriate and able to be adapted easily pending the requirements of the guests.
- It should be ensured via consent condition that all doors fronting the carpark area shall have glazed vision panels to allow for informal surveillance of this space. The architectural prominence of the pedestrian entrance to the office/reception area from the carpark should be enhanced for legibility and wayfinding to be visually distinctive from the other motel unit doors. For best urban design benefit the reception office floor layout should be amended to include surveillance out to both the street and the carpark, however if this is not practicable, as a minimum the door between the carpark area and reception should also be fully glazed for informal surveillance purposes.
- All air conditioning plants and vents shall be screened. Should any plant system need to be installed on the roof they should not be visible from street level.
- A lighting plan shall be produced for safety and amenity purposes subject to urban design approval. All exterior lighting is to comply with the Southern Lights Strategy. Lighting within the carpark area shall incorporate capping in their design so as to be downlit only to reduce light spill upwards.

Landscape

- The proposed fencing along the Dungarvon and Brownston Street boundaries should be reconsidered to reduce the bulk form of the proposal in relation to the street whilst still providing privacy screening. The current design response has a landscaped yard along the street frontage for outlook purposes only and not for guests to use. Whilst I consider that outdoor areas in studio units intended for motel use may not be necessary in this location due to the close proximity of the site in relation to parks and reserves of high amenity that the visitors can use and enjoy, the planting buffers provide minimal wider amenity, which underutilises the space and provides minimal contribution to street amenity. This problem can be easily resolved by shifting the screening closer to the units within the proposed landscaping area so that both the guests and the public can appreciate the planting measures. It is also recommended that the *Sophora Microphylla* be planted on the street side of the proposed fencing to reduce the potential visual monotony of the screening and further detract from the effects of site standard infringements.
- The trees and plants at time of planting should be of a minimum size to provide immediate effect to improve amenity and reduce and visually soften the built form effects of the proposal. I recommend that the tree species *Pseudopanax crassifolius* shall have a planting size grade of no less than 10l and the *Sophora Microphylla* shall be no less than 25l. The *Sophora Microphylla* shall be crown lifted to 2m above ground once they have reached a height of 4m and all areas of mass planting shall be managed to achieve a closed canopy no more than 1m in height. This is to maintain a clear public sight line under the canopy of trees to the building and surrounds to ensure informal public visual surveillance of the site.
- All planting shall be maintained as per the approved landscape plan (includes planting, management and maintenance plans). If any plant shall die or become diseased it shall be replaced within 6 months as per the certified landscape plan.
- A step free, firm, slip resistant pathway is to be installed from either the accessible car parking space to the main building and common areas of the building providing a minimum clear width of 1200mm (*Universal Access*).
- Step free, firm slip resistant paths are to be installed where links to the public footpaths on Dungarvon and Brownstown Streets have been shown. The removal of the separated paver treatment across the grassed berm areas shall be removed and replaced with exposed aggregate. (*Universal Access*)
- The *Griselinia littoralis* hedge identified on the landscape plan immediately adjoining the disabled carpark (Carpark 1) is to be removed to improve accessibility measures and sitelines into the carpark from the street in order to improve CPTED and Universal access.

Should you wish to discuss the content of this memorandum or discuss anything further on this application please contact me.

Yours sincerely,

Erin Quin
Urban Designer /Planner
[BPlan (Hons.)MUrbDes]
Vivian+Espie Limited, on behalf of
Planning & Development
Queenstown Lakes District Council.

Direct Dial: 03 441 4189
Email: erin@vivianespie.co.nz

APPENDIX 5 – ACOUSTIC REPORT

20 July 2018
Ref: 1401101

Queenstown Lakes District Council

By email: simon.childs@qldc.govt.nz

Dear Simon

Subject: RM180133 Acoustics comments

Introduction

Chiles Ltd has been engaged by the Queenstown Lakes District Council (QLDC), to review noise effects associated with an application for resource consent (RM180133) to construct and operate visitor accommodation at 86 Brownston Street, Wanaka. This review has been conducted from review of the operative district plan and the Resource consent application dated 29 January 2018. A site visit has not been conducted for this review, but the author is familiar with the area.

Operative district plan

The site and neighbouring sites are in a Low Density Residential Zone. For non-residential activity, such as visitor accommodation, noise limits of 50 dB L_{Aeq} apply during the day (0800h to 2000h) and 40 dB L_{Aeq} at night. Construction sound is required to comply with NZS 6803.

Construction activity

There are no unusual construction activities evident from the application, and it should be practical for construction noise to comply with NZS 6803. While there is likely to be temporary disturbance for neighbours, this should be to a reasonable degree as expected for normal urban development. A construction noise and vibration management plan is an appropriate control mechanism.

Visitor accommodation activity

Visitor accommodation includes essentially the same sound sources as residential activity in this zone, although these can be of a different frequency or intensity. Sound sources include: people, music, visitor vehicles, service vehicles and building service equipment such as heat pumps.

Building services equipment can be designed to comply with the noise limits with standard measures.

Occasional visitor vehicle movements should comply with daytime and night-time noise limits if there is a fence/wall around on the boundaries of the car park with 37 Dungarvon Street and 90 Brownston Street (youth hostel). The fence/wall should be a solid construction at least 2 metres high without gaps. It is understood from the application that there is existing vegetation on the boundaries that provides visual amenity. If a fence/wall cannot be constructed without removing that vegetation then it is recommended that neighbours are consulted to determine their preferences as the visual amenity of the existing planting may be more important to them than noise reduction that would be provided by a fence/wall. However, this would result in a non-compliance with the night-time noise limits.

Service vehicles and activities such as commercial rubbish collection and cleaning should comply with daytime noise limits but may exceed night-time limits. The fence/wall around the car park should extend past the rubbish/service area shown on the drawings.

People inside buildings with doors and windows closed should comply with daytime and night-time noise limits. People talking quietly outdoors should comply with the daytime noise limit but not the night-time noise limit. Music played outdoors, or indoors with doors open, would generally not comply with daytime or night-time noise limits.

If the visitor accommodation were subject to appropriate controls to maintain compliance with noise limits, the resulting noise effects should be acceptable. The sound would be of the same character as residential activity but would be controlled to a more stringent standard than residential activity.

Conditions

If consent is granted, it is recommended that conditions should be imposed requiring the following.

- A 2 metre high solid fence/wall with no gaps must be constructed between the driveway/car park/service area and 37 Dungarvon Street/90 Brownston Street.
- Construction must be in accordance with a Construction Noise and Vibration Management Plan and in compliance with NZS 6803.
- Sound from building services equipment must comply with a noise limit of 40 dB L_{Aeq} , to be verified by an acoustics specialist prior to occupation of the buildings.
- Visitor accommodation activities must comply with the district plan noise limits.
- Service vehicles/activity must occur only between 0800h and 2000h.
- Loudspeakers or music must not be played outside visitor accommodation at any time.
- Visitor accommodation doors and windows must be kept closed between 2000h and 0800h and at all times music is played inside.
- Visitor accommodation guests must be informed of and sign agreement to comply with noise restrictions prior to occupation. Prominent signage of requirements to be displayed in each unit.

Conclusions

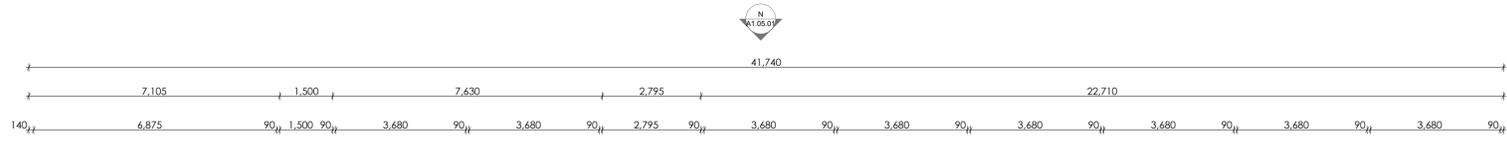
Potential noise effects from a proposed visitor accommodation development have been reviewed. Construction activities should have acceptable noise effects with no specific controls required. If visitor accommodation activities are subject to conditions as outlined above, these should comply with the district plan noise limits and also have acceptable noise effects in this environment.

Yours sincerely

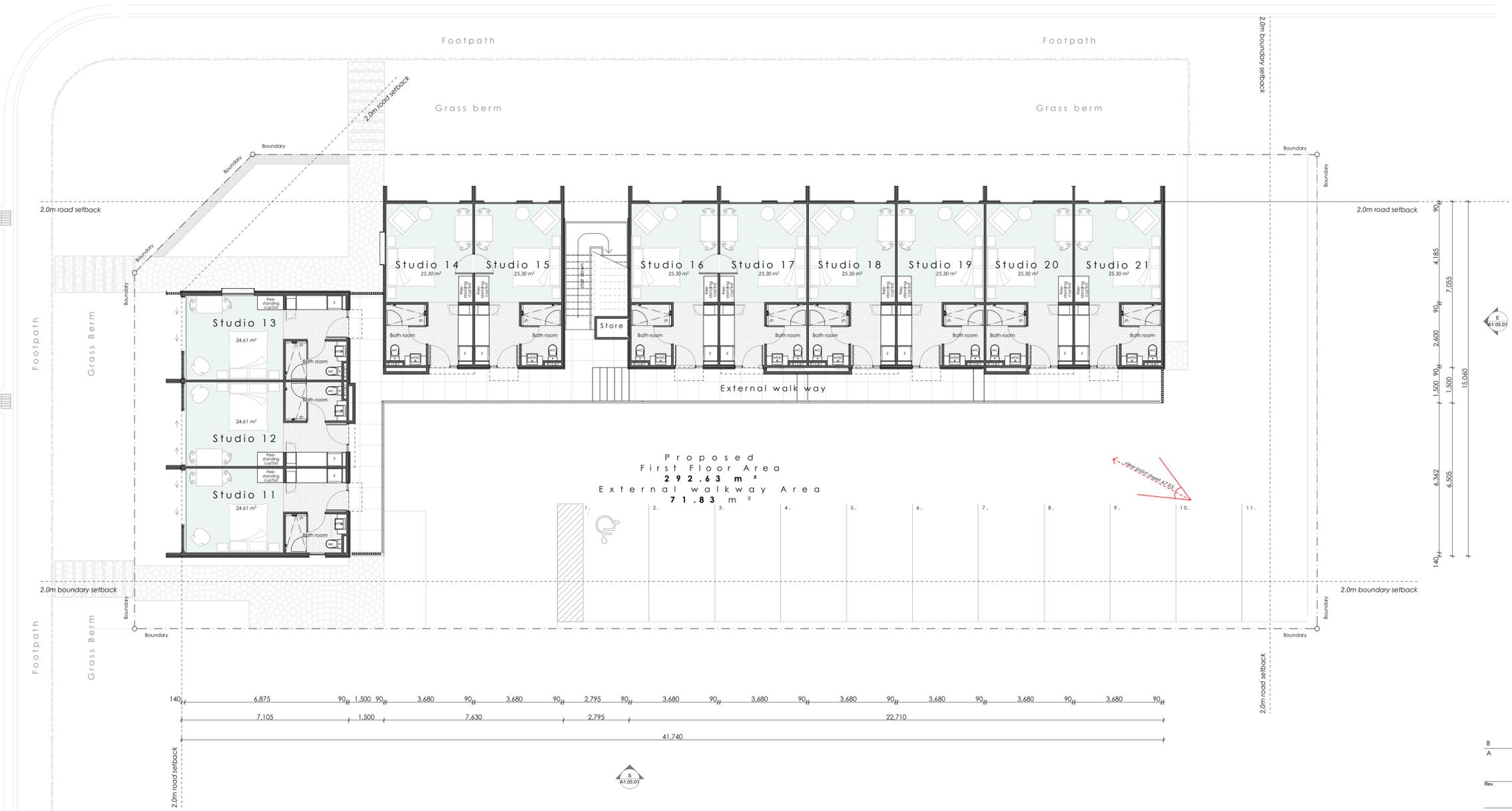
Chiles Ltd



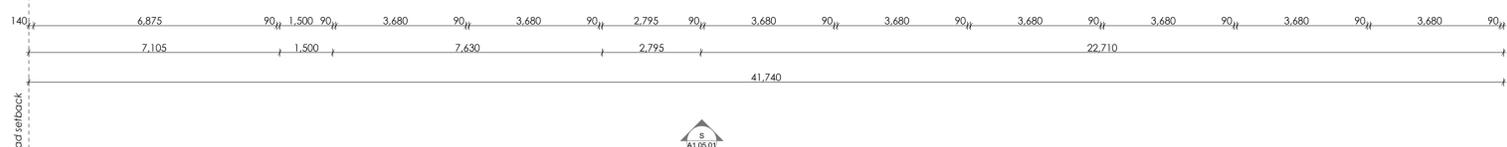
Dr Stephen Chiles
stephen@chiles.co.nz
027 519 1555



Dungarvon Street



Proposed
First Floor Area
292.63 m²
External walkway Area
71.83 m²



PLAN: LEVEL 2 FLOOR scale: 1:100 on A1
scale 1:200 on A3

B	10.06.2019	Carpark Layout updated
A	02.05.2018	True location of footpath & grass berm included. Grounded heights to all units entry doors. Carpark layout & numbers landscaping below to be confirmed. Additional landscaping to carpark below.
Rev.	date	issue

drawing status: Preliminary / Concept issue

drawing title: PLAN: LEVEL 2 FLOOR

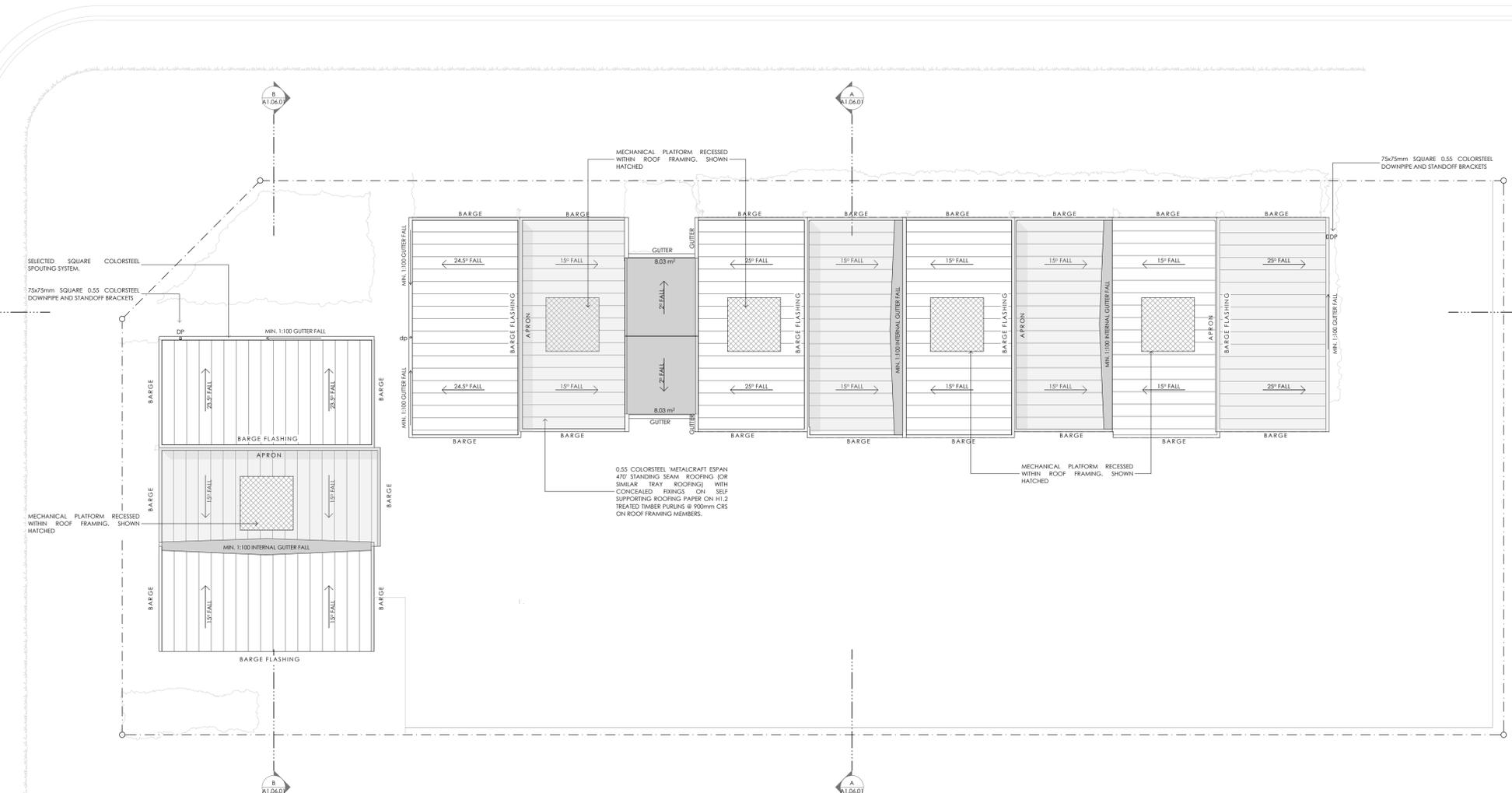
layout number: A1.02





Dungarvon Street

Brownston Street



PLAN: ROOF scale:1:100

B	10.06.2019	Carpark layout updated
A	02.05.2018	True location of footpath & grass berm included. Roof drain orientated. Mechanical platform shown recessed within roof framing. Landscaping below to be confirmed. Additional landscaping to carpark below.
Rev.	date	issue

drawing status: Preliminary / Concept issue

drawing title: PLAN: ROOF

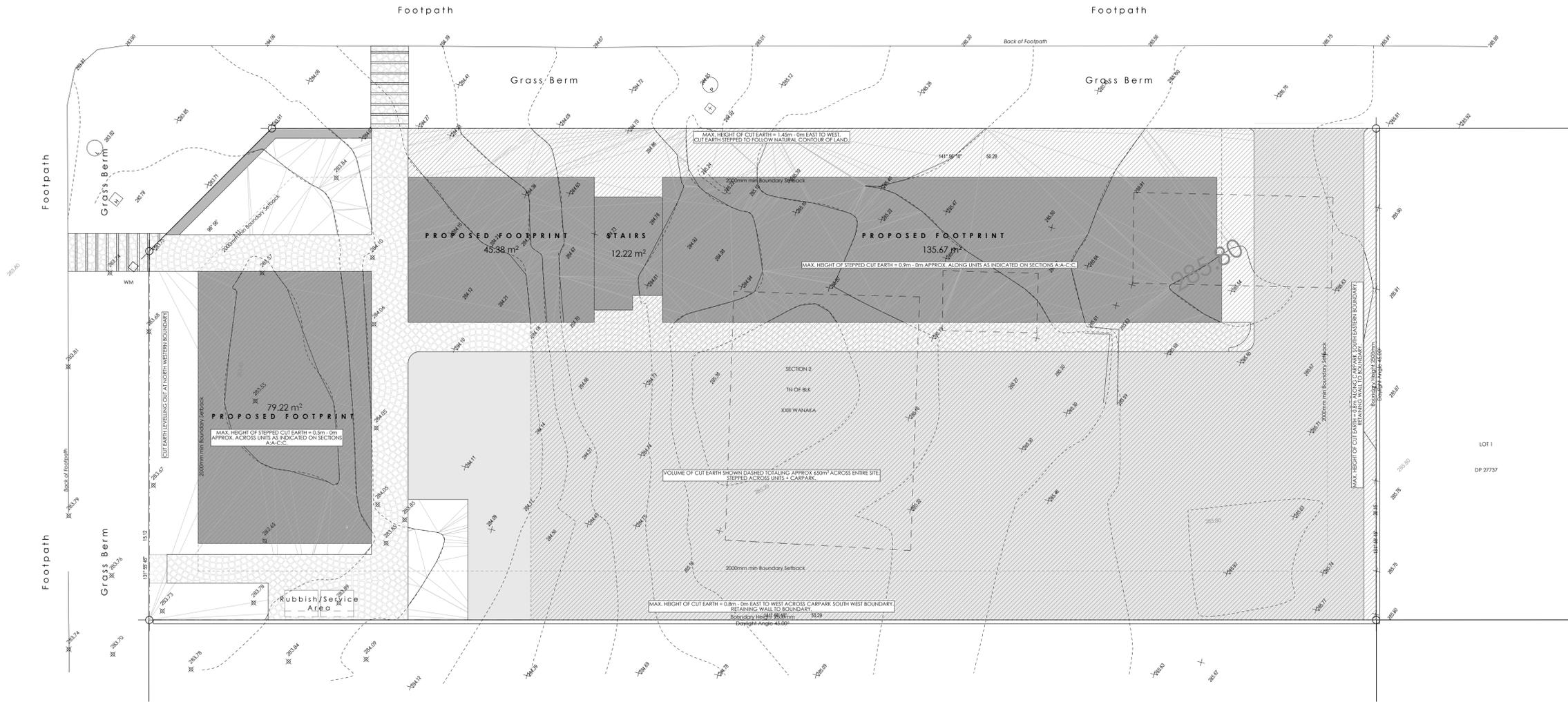
layout number: A1.03





Dungarvon Street

Brownston Street



PLAN: SITE WORKS scale:1:100

Rev.	date	issue
drawing status:		
Preliminary / Concept issue		
drawing title:		
PLAN: SITE WORKS		
layout number:		
A1.04		



NOTES:
 the contractor shall verify all dimensions on site before commencing work. Written dimensions are to take priority over scaled dimensions. All plans are to be read in conjunction with the specification and engineer's details if applicable.
 This drawing is copyright. Under no circumstances may this drawing be reproduced in any form without the written permission of Walker Architecture.

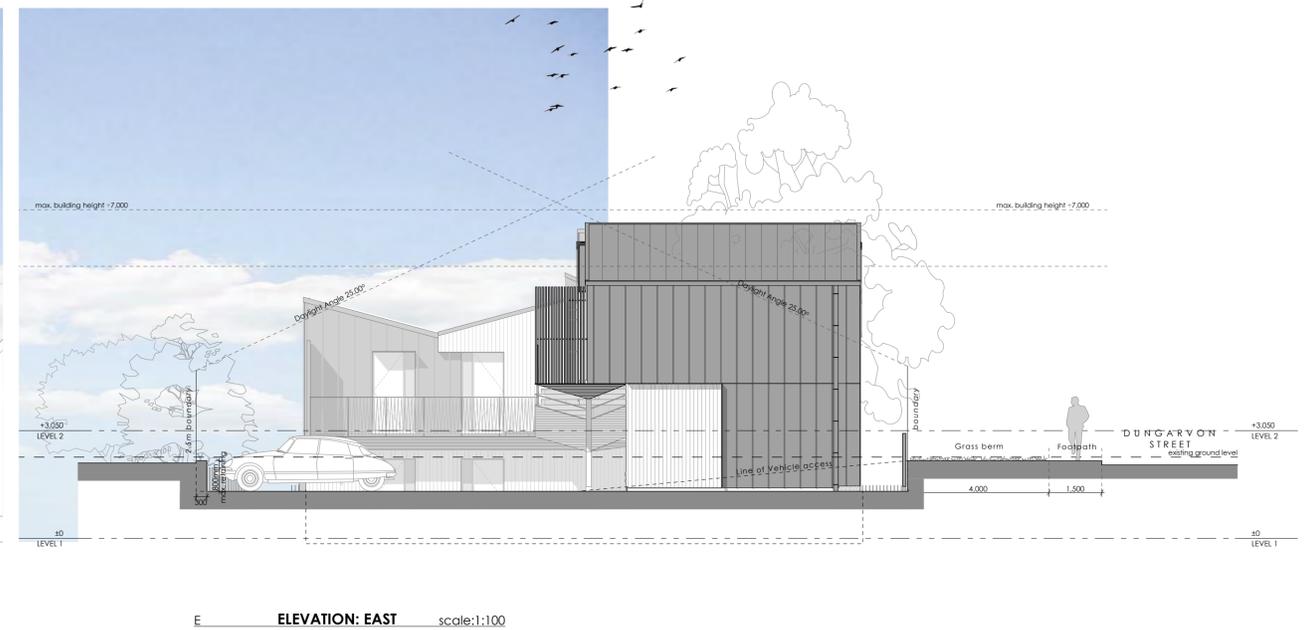
MOTEL CONCEPT
 project # 17.1042



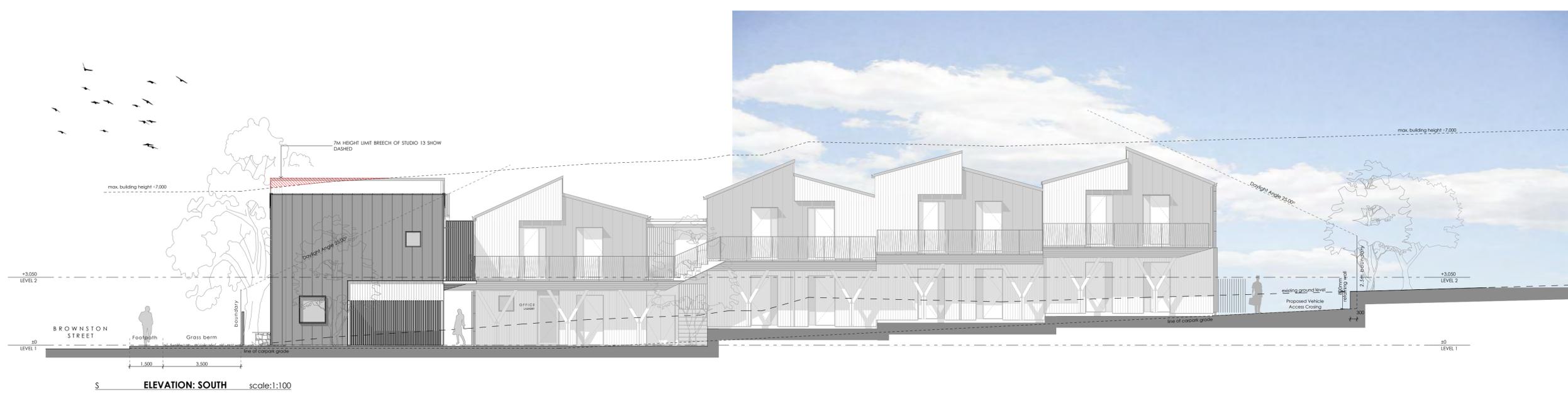
N **ELEVATION: NORTH** scale:1:100



W **ELEVATION: WEST** scale:1:100



E **ELEVATION: EAST** scale:1:100

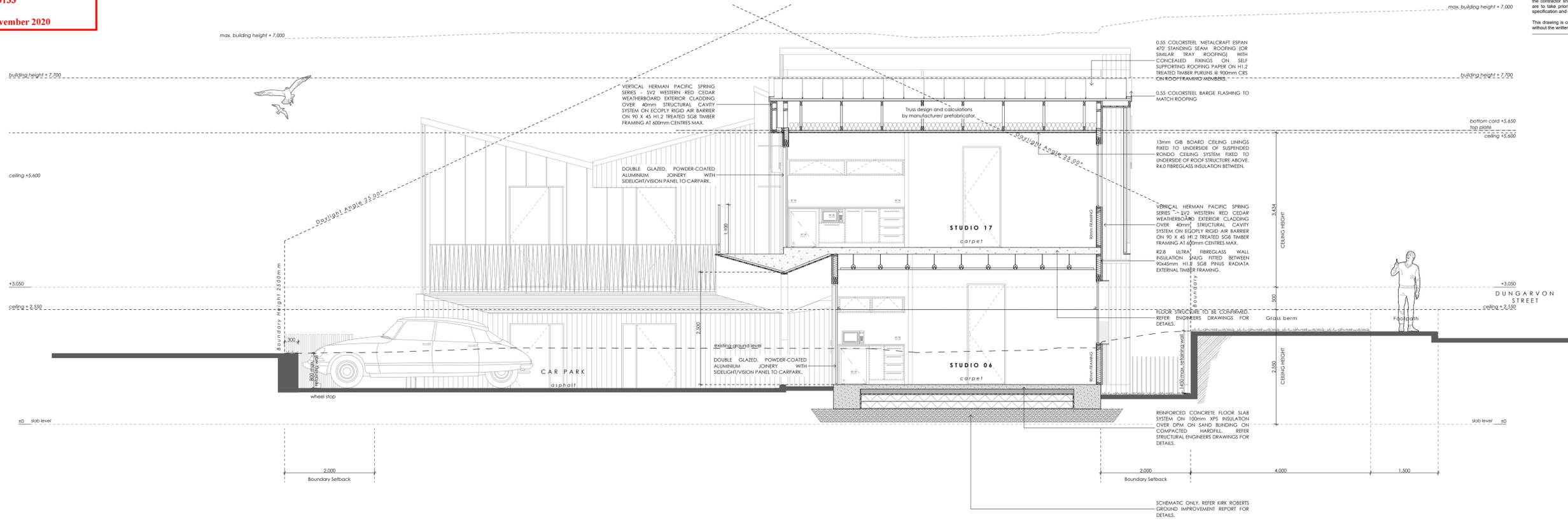


S **ELEVATION: SOUTH** scale:1:100

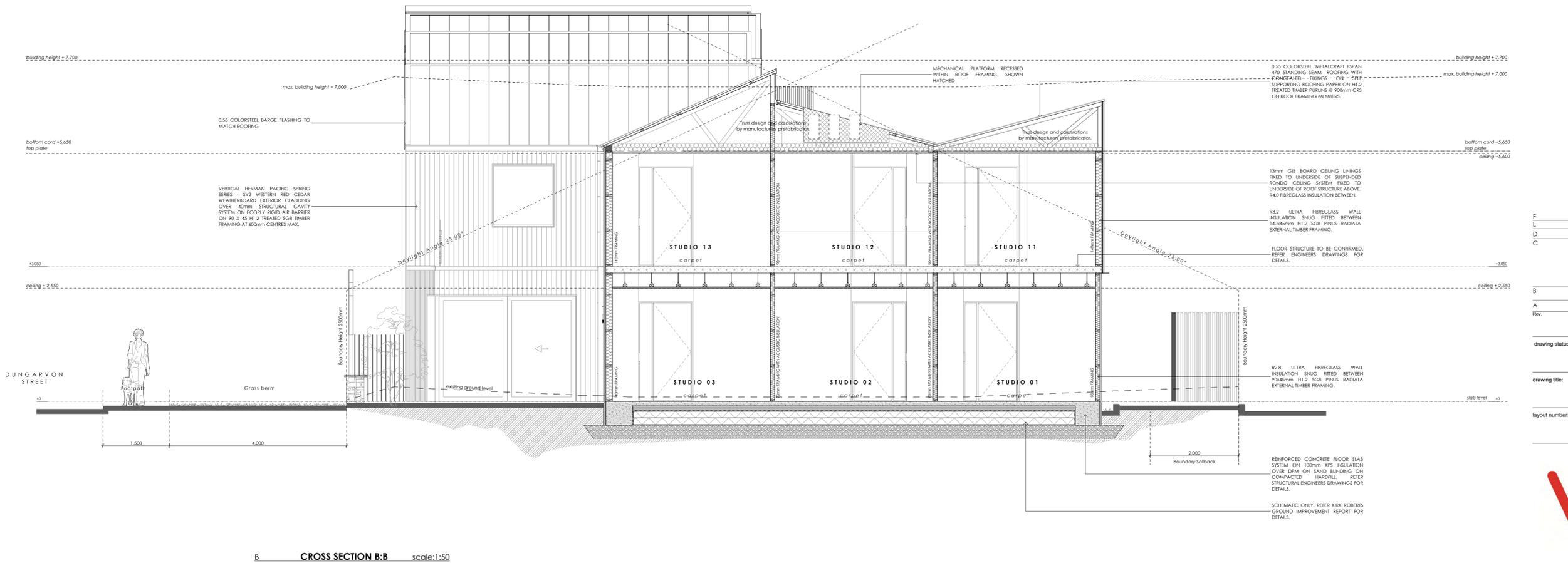
Rev.	date	issue
G	03.07.2019	Retaining wall amended
F	10.04.2019	East Elevation - Revised retaining wall
E	13.04.2018	Building fixed to follow site and below 7m height limit
D	21.05.2018	Existing ground level shown with 7m height limit
C	02.05.2018	Office relocated Road boundary fence and grass berm included Grazed site/light to air entry doors Neighbouring properties included Solar/privacy screen reduced/more permeable Building height reduced
B	02.02.2018	Additional facade plane included Building height amended
A	13.12.2017	Retaining plane amended Max. building height amended

drawing status: Preliminary / Concept issue
 drawing title: ELEVATIONS
 layout number: A1.05.01





A CROSS SECTION A:A scale:1:50



B CROSS SECTION B:B scale:1:50

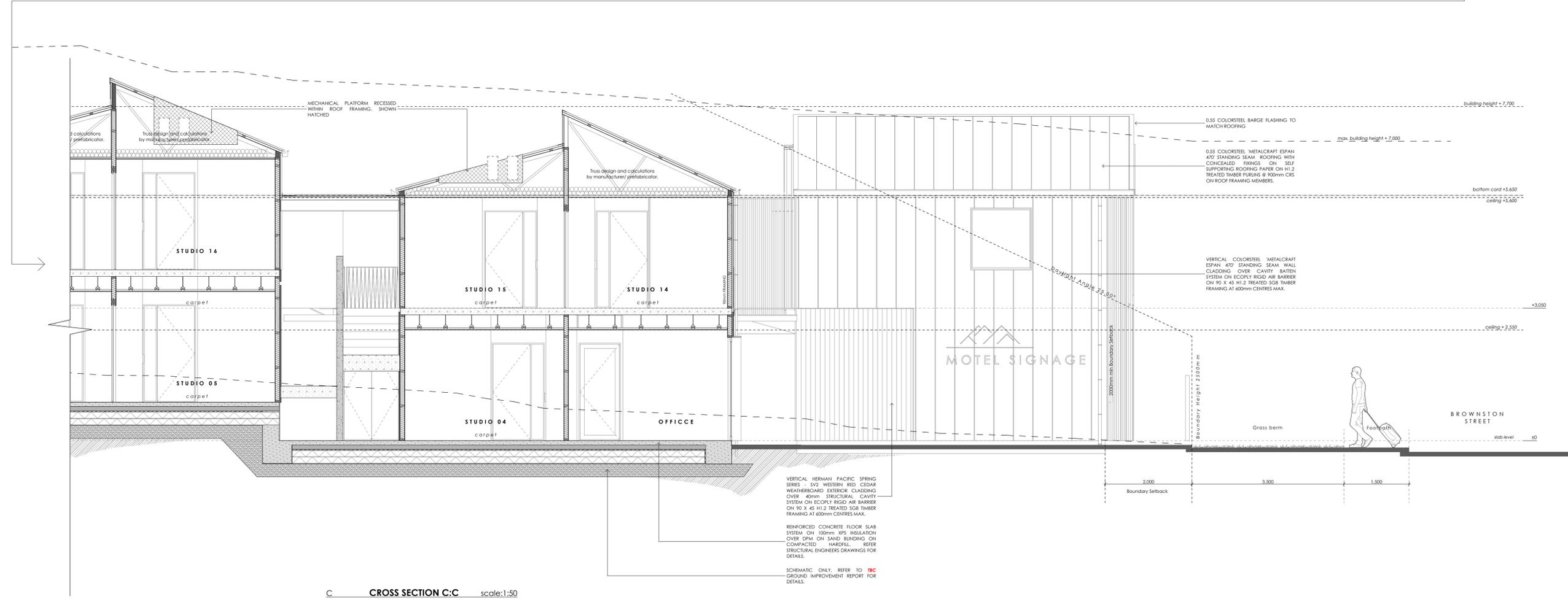
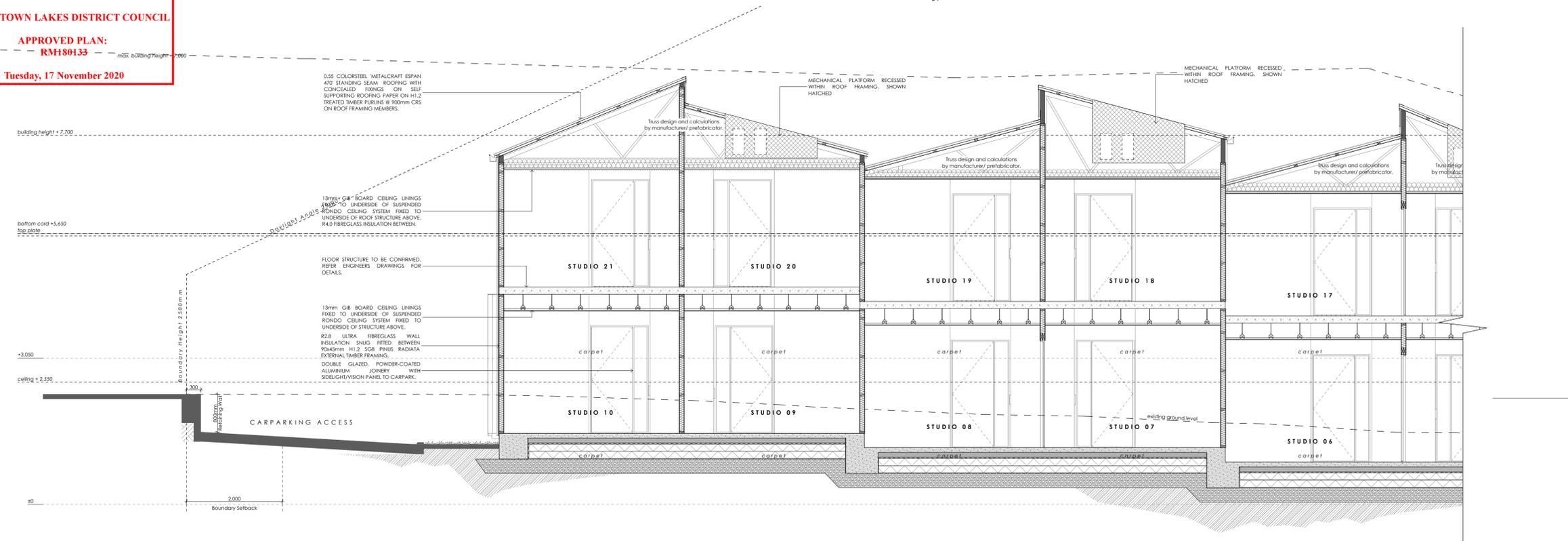
Rev.	date	description
F	10.06.2019	Cross Section A-A: Tiered retaining wall
E	13.06.2018	Building tiered to follow site and sit below 7m height limit.
D	21.05.2018	Existing ground level shown with 7m height limit.
C	02.05.2018	Office relocated. Grassed sideights to all unit entry doors. Stairway screen replaced with permeable. Building height reduced. True location of footpath & grass berm included. Roof pitch amendments Mechanical platform shown recessed within roof framing.
B	02.02.2018	Additional recession plane included. Building height amended.
A	13.12.2017	Recession plane amended. Max. building height amended.
Rev.	date	issue

drawing status: Preliminary / Concept issue

drawing title: SECTIONS A + B

layout number: A1.06.01





Rev.	date	description
E	13.06.2018	Building fixed to follow site on/draft below 7m height limit.
D	21.05.2018	Existing ground level shown with 7m height limit.
C	02.05.2018	Office relocated. Road boundary fence included. Grazed sideights to off unit entry doors. Separatory screen reduced/stone permeable. Building height reduced. True location of footpath & grass been included. Roof pitch amendments. Mechanical platform shown recessed within roof framing.
B	02.02.2018	Additional recession plane included. Building height amended.
A	13.12.2017	Recession plane amended. Max. building height amended.

drawing status: Preliminary / Concept issue
drawing title: CROSS SECTION C
layout number: A1.06.02



C CROSS SECTION C:C scale:1:50

REV	DATE	NOTES
O	10/10/17	FOR REVIEW
A	17/10/17	FOR WANAKA UDP
B	20/03/18	FOR RESOURCE CONSENT
C	10/10/18	FOR RESOURCE CONSENT
D	23/07/2019	FOR RESOURCE CONSENT

QUEENSTOWN LAKES DISTRICT COUNCIL

APPROVED PLAN:

Sloping garden bed with *Pseudopanax crassifolius* underplanted with a mixture of tussocks including *Chionochloa rubra*, *C. rigida* and *Poa cita*

RM180136
Tuesday, 17 November 2020

Schist walls shall retain sloping garden bed.

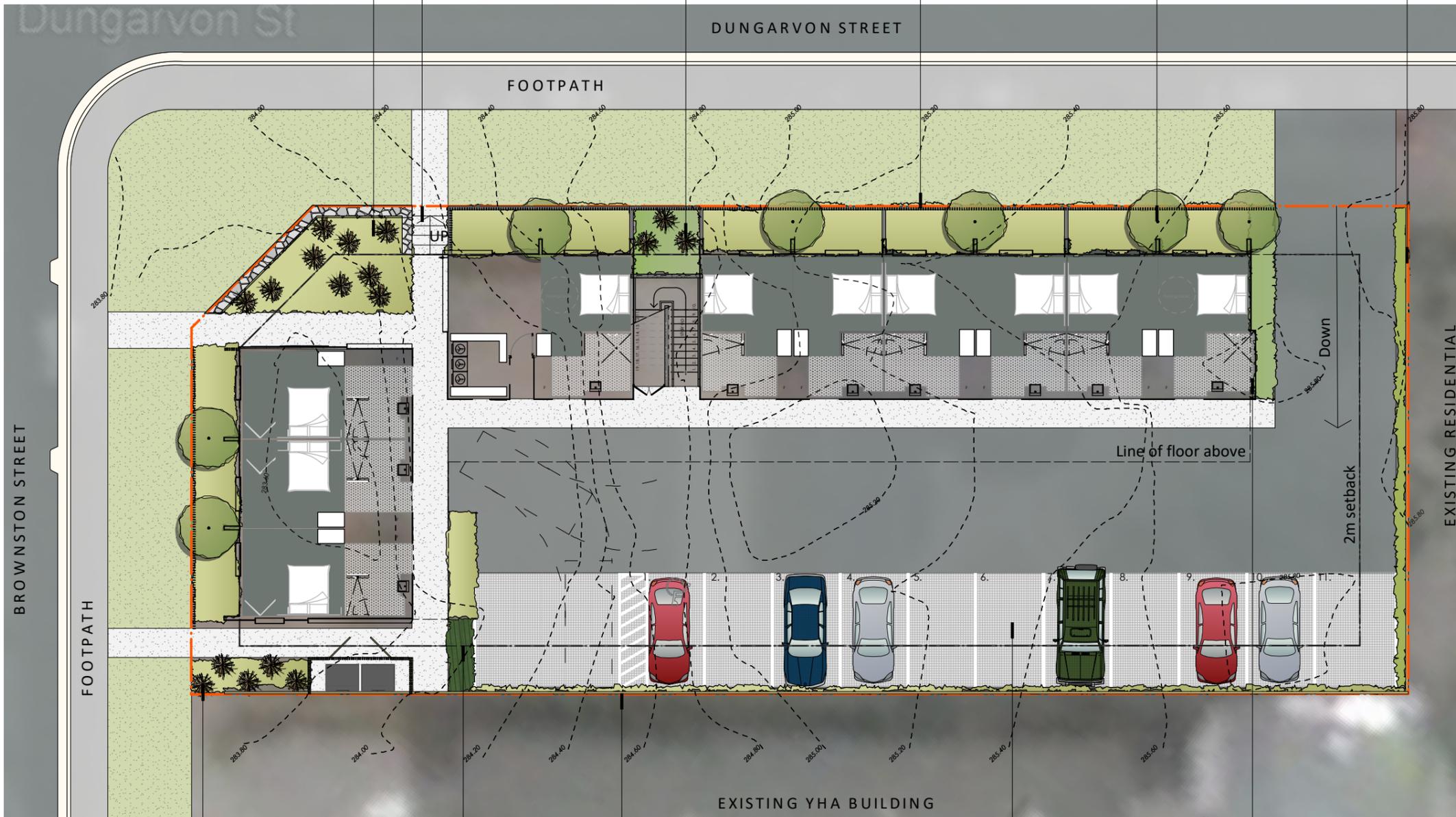
Exposed aggregate pedestrian access. 5x steps from Dungarvon Street access only.

Pseudopanax crassifolius underplanted with *Muehlenbeckia axillaris*

1.2m high timber fence to top of retaining wall. Max height of fence and retaining wall is 1.5m

Sophora microphylla underplanted with a mixture of tussocks including *Chionochloa rubra*, *C. rigida* and *Poa cita*

Proposed timber fence 1.8m high on top of retaining wall with *Clematis paniculata* climber TBC with retaining wall design



NOTE:

- 1). *Pseudopanax crassifolius* planted at a grade of 10L.
- 2). *Sophora Microphylla* shall be planted at a grade of 25L. Once the tree reaches a height of 4m its crown shall be lifted to a height of 2m.
- 3). Tussocks and flaxes shall be planted at sufficient spacing to achieve full coverage.
- 4). *Clematis* planting at base of retaining walls to be confirmed with retaining wall design.



rough & milne landscape architects
 CHRISTCHURCH +64 3 366 3268
 WANAKA +64 3 974 7940
 AUCKLAND +64 27 642 3342
 www.roughandmilne.co.nz info@roughandmilne.co.nz

LANDSCAPE CONCEPT
BROWNSTONE STREET MOTELS
86 BROWNSTON STREET
WANAKA

JOB No.	17044
SCALE	1:150 @ A3
DATE	10/10/17
DESIGNED	EMILY-ROSE DUNN
DRAWN	EMILY-ROSE DUNN
CHECKED	TONY MILNE
STATUS	FOR RESOURCE CONSENT
DRAWING No.	REVISION
L 1.0	D
SERIES	
1 of 2	

Pseudopanax crassifolius underplanted with *Chionochloa rigida*

Griselinia littoralis 'Broadway mint' hedge maintained at 2m high

Proposed timber fence 1.8m high on top of retaining wall with *Clematis paniculata* climber TBC with retaining wall design

Phormium cookianum 'Emerald Green' planted along accessway.

Gobi block carpark bays

Line of floor above

Down
2m setback

EXISTING YHA BUILDING

DUNGARVON STREET

FOOTPATH

BROWNSTON STREET

FOOTPATH

EXISTING RESIDENTIAL

QUEENSTOWN LAKES DISTRICT COUNCIL

**APPROVED PLAN:
RM180133**

Tuesday, 17 November 2020

REV	DATE	NOTES
O	10/10/17	FOR REVIEW
A	17/10/17	FOR WANAKA UDP
B	20/03/18	FOR RESOURCE CONSENT
C	10/10/18	FOR RESOURCE CONSENT
D	23/07/2019	FOR RESOURCE CONSENT



1 Landscape Elevation: North
Scale: 1:150



2 Landscape Elevation: West
Scale: 1:150

rough & milne landscape architects

CHRISTCHURCH +64 3 366 3268
 WANAKA +64 3 974 7940
 AUCKLAND +64 27 642 3342
 www.roughandmilne.co.nz info@roughandmilne.co.nz

**LANDSCAPE ELEVATION
BROWNSTONE STREET MOTELS
86 BROWNSTON STREET
WANAKA**

JOB No.	17044
SCALE	1:150 @ A3
DATE	10/10/17
DESIGNED	EMILY-ROSE DUNN
DRAWN	EMILY-ROSE DUNN
CHECKED	TONY MILNE
STATUS	FOR RESOURCE CONSENT
DRAWING No.	REVISION
L 2.0	D
SERIES	
2 of 2	